



Dutton Lane Car Park Redevelopment

Statement of Environmental Effects

Client:
Fairfield City Council

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1 Introduction

This Statement of Environmental Effects (SEE) is submitted on behalf of Fairfield City Council (the Applicant) as part of a Development Application (DA) to redevelop the existing at-grade car park along Dutton Lane, Cabramatta. The existing at-grade car park is located alongside Council's multi-deck car park to the west.

The proposal is to construct a three storey building containing retail on ground floor, commercial uses and a public car park on the first level and a public car park on the second level.

The site is legally made up of the following lots and part lots:

- » Lot 6 and Lot 4 DP 236708
- » Lot 2 DP 215976
- » Lot 3 DP 540495
- » Lot 3 DP 216870
- » Lot 3 DP 217606
- » Lot 8 DP 238484
- » Lot 2 DP 534197
- » Lot 2 and Lot 1 DP 539611
- » Lot 1 and Lot 2 DP 570243
- » Lot 1 DP 532304
- » Lot 2 DP 548450.

The value of the proposed development is estimated at \$16.3 Million. The consent authority is the Sydney West Joint Regional Planning Panel.

1.1 Summary of the proposed development

The DA formally seeks approval under part 4 of the *Environmental Planning and Assessment Act 1979* (the EP&A Act) for:

- » Removal of an existing at-grade car park and ancillary structures (including public toilet facility)
- » Construction of a commercial/retail development with car parking over 3 levels, being:
 - > Ground level: comprises of retail floor area including vehicular access to the upper levels
 - > First level: comprises commercial space which is proposed to be used for community uses in the short term, as well as car parking areas
 - > Second level: comprises of car parking area
- » The development will result in:
 - > an overall net lettable area of 2,995 sqm of retail space
 - > an overall net lettable area of 505 sqm of commercial space
 - > a maximum height of 10.85 metres

- > 275 car parking spaces; and
- » Subdivision of the Council-owned lots in the Dutton Lane precinct to create an orderly lot arrangement
- » Modification to the loading bay arrangements at Dutton Lane and the Hughes Street public car spaces
- » Modification to intersection arrangements at Dutton Lane/Hughes Street intersection.

The building will comprise of a singular strong colour which will incorporate natural tones and materials to the building to create identity and presence. Stairwells will be designed to be very open and to break up the facade as well as providing security and safety to the area.

This SEE provides an analysis of the site and its context, a description of the proposed development and an assessment of the proposal against the heads of considerations under Section 79C(1) of the EP&A Act.

1.2 Structure of this Report

This SEE is structured as follows:

- » **Section 1** introduces the proposal and provides relevant background information for the application
- » **Section 2** provides a description of the site, its location and surrounding development;
- » **Section 3** contains a description of the proposed development
- » **Section 4** contains an overview of the relevant statutory planning instruments and policies as they apply to the proposal
- » **Section 5** assesses the planning impacts of the proposal pursuant to the heads of consideration under section 79C(1) of the EP&A Act
- » **Section 6** provides a conclusion to this report and summarises the environmental effects of the proposed development.

1.3 Accompanying Documents

This SEE should be read in conjunction with the Architectural Plans, physical model and supporting technical information, which includes:

- » Architectural plans and drawings prepared by Antoniades Architects (Appendix B)
- » Plan of proposed Subdivision prepared by Proust & Gardner Consulting (Appendix C).
- » Traffic Report prepared by Thompson Stanbury Associates (Appendix D)
- » Economic Impact Assessment prepared by HillPDA (Appendix E)
- » Architectural Statement prepared by Antoniades Architects (Appendix F)
- » Stormwater Drainage Plan prepared by itmDesign (Appendix G)
- » BCA Assessment prepared by Steve Watson & Partners (Appendix H).

1.4 Background

Council held an ordinary meeting on the 11th December 2012 in which a motion was unanimously carried to prepare a DA to redevelop the site. It was noted that the decision was made in response to the need for more car parking spaces in the Cabramatta City Centre and the better utilisation of its asset

As landowner/applicant, Fairfield City Council has commissioned an external planner to assess the application. The application will be assessed by the external consultant and ultimately be determined by the Sydney West Joint Regional Planning Panel (JRPP).

1.4.1 Development Advisory Meeting

A Development Advisory Meeting was held on 18 June 2013 at Fairfield City Council to discuss the proposed development in concept stage. The meeting was attended by:

- » Paul Grech and Peter Jobson (GLN Planning – consultant planner for Fairfield City Council)
- » Andreas Antoniadis and Keiko Mino (architect)
- » Jenny Rudolph (planning consultant)
- » Morgan Stanbury (traffic consultant)
- » Sunnee Cullen, Grant Milineou and Ody Murlianto (for Fairfield City Council as the consent authority).
- » Hao Dang (for Fairfield City Council as the applicant)

Table 1 below outlines the issues raised by Council and how this DA responds.

Table 1 Response to issues raised at Development Advisory Meeting

No.	Issue raised by Council	Comment
1	The building will maintain a height of 10m with the exception of the stairwells which exceed 10m. The additional proposed height of the stairwells will need further consideration and justification in relation to a variation to the relevant development standard.	Addressed. The sections of the building that encroach the 10 metre height limit are justified against Clause 4.6 of Fairfield LEP 2013.
2	A copy of the Economic Report prepared by HillPDA should be included with the DA documentation to gain an understanding of the rationale behind the proposed development.	Addressed. Refer to Appendix E.
3	Traffic Report will need to take into account the following matters: <ul style="list-style-type: none">» The heavy use of the Dutton Lane Car park including the access ring road, for loading/unloading of trucks, servicing of shops, drop-off/pick up of shoppers etc.» The heavy pedestrian use of Dutton lane» Justification for how the multiple activities (pedestrian, vehicle, trucks, etc) will operate.» If any loading bays are to be affected, consideration	Addressed. Refer to Appendix D.

No.	Issue raised by Council	Comment
	<p>should be given to providing replacement loading areas within the site or alternate nearby locations within the centre.</p> <p>» Explanation of vehicular access and pedestrian arrangements in and around the site.</p>	
4	It was considered prudent that Council conduct public consultation prior to the submission of the application.	Addressed. Section 5.4.2 of this SEE describes the public consultation process undertaken prior to lodgement of the DA.
5	Consideration of the need to replace the existing car parking (public) bays as well as the public facilities (toilets) to be replaced.	Addressed. Refer to Section 3.1.3 of this SEE.
6	Cabramatta Town Centre Development Control Plan (DCP) No. 5/2000 states that 30% of parking should be provided on site. Any variation from the 30% standard of the DCP will need to be justified in accordance with the Section 94 Plan.	Addressed. Refer to Appendix A of this SEE.
7	Consideration of the objectives of the B4 zone and the relevant provisions of Cabramatta DCP 5/2000 to demonstrate that the proposal is consistent with the planning intentions of the DCP Precinct 2.	Addressed. Refer to Appendix A and Section 4.1.2 of this SEE.
8	Consideration to future air-bridges that may join the proposal and where these could be located.	Air bridges are not proposed as part of this DA, however the design does not preclude the provision of these features in the future.
9	The Pailou Gates, located within Freedom Plaza, are heritage listed. The applicant should consult with Council's Heritage advisor to determine what (if any) information should be provided to address this matter.	Addressed. Information was provided to Council's Heritage Advisor about the proximity of the site to the heritage item. The Advisor subsequently indicated that no further details are required as the proposal has no impact on heritage.
10	<p>In addition to the above, the DA will need to address the following:</p> <p>» Compliance with the provisions of Fairfield LEP 2013, relevant DCPs in particular Cabramatta DCP 5/2000 and the Fairfield S94 Contributions Plan.</p> <p>» Details of the proposed architectural treatment and materials and colours.</p>	<p>» Addressed in Section 4.1.2 this SEE.</p> <p>» Addressed in Section 3.1.6 this SEE. Refer to Design Statement attached in Appendix F.</p>

No.	Issue raised by Council	Comment
	<ul style="list-style-type: none"> » Consultation with Cabramatta Police prior to lodging the application. » Details of waste facilities should be provided. » Location of plant equipment, extraction systems etc. as the top level will contain a carpark area. » Details of toilet numbers will need to be considered. There will be a need to replace existing toilets and provide toilets to the development. » Compliance with the Building Code of Australia to be demonstrated. » The Applicant shall provide the Capital Investment Value (CIV) for the proposal. 	<ul style="list-style-type: none"> » Addressed in Section 5.3.2 this SEE. » Addressed in Section 3.1.7 this SEE. » Refer to Architectural Plan (Appendix B). » Addressed in Section 3.1.3 this SEE and Architectural Plans. » Addressed in Section 3.1.11 this SEE. » Refer to DA form.
11	<p>The following documents were required:</p> <ul style="list-style-type: none"> » Development Application (DA) form. » A Statement of Environmental Effects which provides full details of your application. » Traffic and Parking Impact Assessment. » Economic Impact Assessment Report. » Heritage Impact Report – upon consultation with Council’s Heritage Advisor. » Full architectural Plans including a site and urban design analysis » Stormwater drainage concept plan and Works-As-Executed Plan for the existing stormwater drainage system if available. » Waste Management Plan » Section 149(2) and (5) certificate » Owners Authorities » The relevant DA fee and advertising fee » Electronic copy of all documentation to be submitted. 	<ul style="list-style-type: none"> » Provided » Provided » Provided » Provided » Advice from Council’s Heritage Branch indicated this would not be required. » Provided » Provided. Refer to Stormwater Drainage Plan prepared by itmDesign (Appendix G) » As there is only a small component of demolition, the main focus of the WMP will be on construction materials. This is proposed to be incorporated into the Construction Management Plan. » Provided with DA material. » Provided – see DA form » Provided – see DA form » Provided

2 Site Location and Context

2.1 Site Location

The subject site is located along Dutton Lane (refer to Figure 1), which is bounded by Hughes Street, Park Road, John Street and Hill Street.

Dutton Lane is a one-way ring-road accessed via Hughes Street to the north.

The subject site is occupied by a Council-owned at-grade car park and toilet block which operates between the hours of 7.30am and 5.00pm.

Immediately to the west of the site is a 4 level multi-deck Council (public) car park. The proposal does not alter the operation of the multi-deck car park. However, a vehicle link to the existing multi-deck car park to the west is proposed.

A mix of commercial/retail uses surrounds the site, primarily small shops such as fruit shops, butchers and the like. A two storey complex on the northern side of Dutton Lane, to the north of the at-grade car park, contains a restaurant. In the wider surrounding area, three to four storey residential flat buildings are located along Hill Street and Cabramatta Railway Station is about 200 metres to the east of the site.

Located in the heart of Cabramatta, the site is within walking distance of the railway station and the main shopping/commercial strip in the town centre. It is strategically located to provide important car parking facilities and some new retail and commercial development to complement the existing uses. It is also ideally suited to accommodate an extension to the existing pedestrian and public domain areas which are currently focused along Park Road to the east of the site.

Figure 1 Aerial map of subject site

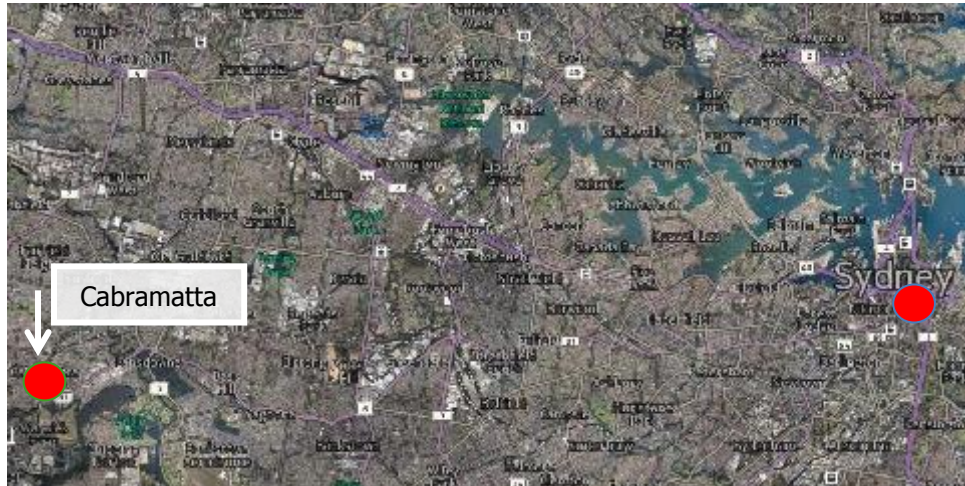


Source: Nearmaps

2.2 Cabramatta in Context

Cabramatta is a suburb located in south-western Sydney, in the state of New South Wales, Australia. Cabramatta is located 30 kilometres south-west of the Sydney central business district (CBD), in the local government area of the City of Fairfield (Refer to Figure 2).

Figure 2 Cabramatta context



Source: Nearmaps

Cabramatta is a highly multicultural suburb. The 2011 Census conducted by the Australian Bureau of Statistics identifies that nearly three quarters (69%) of the Cabramatta population were born overseas and 87.7% of households in Cabramatta speak more than one language.

The Cabramatta CBD, the focal point of Cabramatta is a bustling centre for shopping, community celebrations, tourism and cultural activities. Today, Cabramatta represents a cultural focal point for Sydney, with much of the city's population frequently visiting Cabramatta for a strong taste of South East Asia Flavours.

The town centre has largely evolved around this principle, with many retail premises now being occupied by crowded eateries, and the public spaces around the town centre being occupied by members of the community and visitors from other parts of Sydney, seeking an area to sit and embrace the cultural ambience.

Figure 3 Freedom Plaza



Source: Sheila Smart Photography (sourced through Google Images)

2.3 Site Description

An aerial photo of the site is shown at Figure 1, and site analysis enclosed with the Architectural Plans.

As the entire Dutton Lane precinct is in the ownership of Fairfield City Council, the proposed new lot boundaries have been used to determine the site area and boundary lengths. Currently numerous lots of various sizes make up the Dutton Lane precinct. Figure 4 illustrates the boundaries of the "subject site" applied for the purpose of determining FSR and the like.

Illustrated as Area 4 in Figure 4, the site has an area of 5,110 sqm and is rectangular in shape. It has three frontages to Dutton Lane (to the north, east and south) and a fourth frontage (west) which adjoins the existing multi-deck car park to the west.

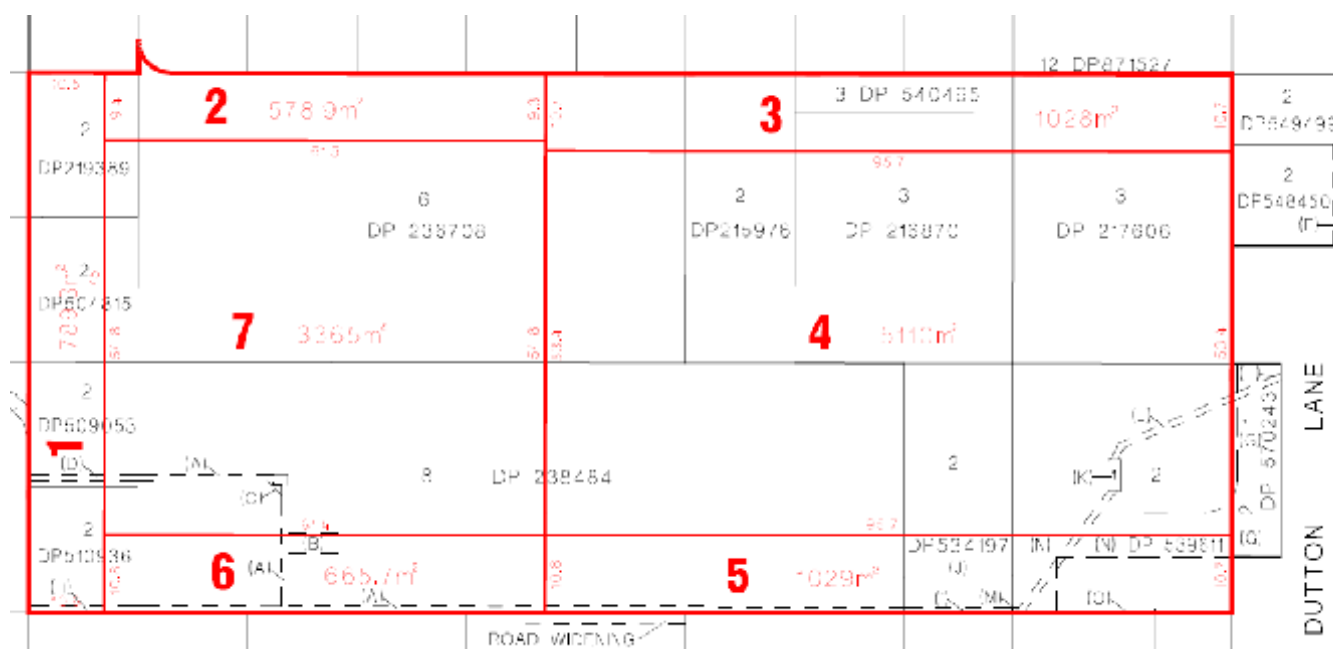
As shown in the enclosed survey plan, the land has a slight fall from northeast (RL 18.60) to south west (RL 20.80).

A description of the site boundaries is provided in Table 2 below.

Table 2 Description of site boundaries

Boundary	Length (metres)
Northern	95.7m
Southern	95.7m
Eastern	53.4m
Western	53.4m
Total site boundary length	298.2m

Figure 4 Site boundaries



Source: Proust & Gardner Consulting Pty Ltd

2.3.1 Site ownership and lot identification plan

The site is currently owned by Fairfield City Council and consists of numerous allotments. Dutton Lane is primarily under the ownership of Fairfield City Council held in fee simple and classified as operational land under Fairfield LEP Amendment 94 on 24th September 2004. The part of Dutton Lane which is classified as a public road is located at the eastern end as shown in DP 570243.

The existing lots vary in size and shape, therefore part of the proposal is to consolidate the lots so as to create a coherent lot layout. The development site occupies different sized proportion of different existing lots.

The site currently comprises several allotments as shown in Figure 5. The proposed development does not affect the entirety of all lots, and will only involve development (or subdivision) across part of the lots.

The lots include:

- | | |
|---------------------------------------|-----------------------|
| » Lot 6 DP 236708 (identified as "A") | » Lot 2 DP 539611 (I) |
| » Lot 8 DP 238484 (B) | » Lot 1 DP 539611 (J) |
| » Lot 2 DP 534197 (C) | » Lot 1 DP 570243 (K) |
| » Lot 2 DP 215976 (D) | » Lot 2 DP 570243 (L) |
| » Lot 3 DP 216870 (E) | » Lot 1 DP 532304 (M) |
| » Lot 3 DP 217606 (F) | » Lot 2 DP 548450 (N) |
| » Lot 4 DP 236708 (G) | » Lot 2 DP 549499 (O) |
| » Lot 3 DP 540495 (H) | |

Figure 5 Lot Identification Plan



Source: Six Spatial Exchange

2.3.2 Existing car parking and loading facilities

The existing Dutton Lane precinct is serviced by a series of car parking areas and loading bays.

Existing loading bays are provided along the perimeter of the site along Dutton Lane, and are governed by sign post restrictions.

The existing loading bay arrangement is describes as follows:

- » **Eight** existing 90 degree bays located within the north-eastern corner governed by 'Loading Zone – Vehicles Under 6m' signage
- » **Three** existing 90 degree bays located in the eastern section of the site governed by 'Loading Zone' signage
- » **Six** existing 90 degree bays located in the south-eastern corner governed by 'Truck Zone' signage
- » **Three** existing 3.5m wide parallel bays governed by 'Loading Zone' signage located on the southern side of Dutton Lane in the south eastern corner
- » **Six** existing 3.5m wide parallel loading bays governed by 'Loading Zone' signage located on the southern side of Dutton Lane in the south western corner
- » There are also three loading bay areas on the northern side of Dutton Lane, to the north of the proposed site, which are proposed to be retained as existing.

Parking for private vehicles on the site is currently provided within the at-grade car park. The existing car parking facility provides 157 parking spaces that are accessed via the existing multi-deck car park.

2.3.3 Existing development on the site

The site does not currently have any significant structures. It is currently occupied by an at-grade car parking area that is asphalted.

The only existing buildings on site are male and female toilet facilities and a small office/booth structure alongside the toilet blocks. The facilities operate between the hours of 7.30am and 5.00pm (Refer to Figure 6 and Figure 7).

Signposts associated with the car parking area, and various railings and bollards, are also located across the site and designed to assist in the operation of the car park.

Figure 6 Existing structures on the subject site



Source: Google Street view

Figure 7 Existing at grade car park



Source: Google Street view

2.3.4 Surrounding development

The site is located within a mixed-use area of Cabramatta Town Centre, generally characterised by:

- » Commercial offices and retail premises
- » Multi-level parking facilities
- » Ground level parking and loading bay facilities.

To the north

A variety of allotments comprising a mix of premises and uses are located immediately to the north of the subject site. At ground level, there are a variety of eateries and food shops, and other general use businesses such as medical practices. Arcades provide pedestrian access from Dutton Lane to Hughes Street to the north.

Above the ground floor is the Cabramatta International Centre, which includes a variety of commercial office suite and medical offices / facilities. A separate portion of the upper level is occupied by a restaurant.

The existing car park is separated from buildings to the north by approximately 12 metres.

Figure 8 Existing development to the north of the subject site



Source: Google street view

To the south

Development to the south of the site consists primarily of loading bays along Dutton Lane and a variety of shops. The ground level is occupied by a mix of retail premises (primarily food related) and some rear entrances associated with retail premises fronting John Street. Arcades provide pedestrian access from Dutton Lane to John Street to the south.

Above ground level, development consists mainly of commercial premises, comprising medical services, tax and finance offices as well as other general businesses.

Figure 9 Existing development to the south of the subject site



Source: Google street view

To the west

Development to the west of the site consists of a four level multi-deck car park facility owned and operated by Fairfield City Council. The facility provides paid public parking. Entry to the car park is from Dutton Lane to the north.

The car park is linked by a raised pedestrian connection to retail premises located to the south of the site.

The subject site (which is proposed to accommodate development) is setback approximately 2 metres from the existing multi-deck car park.

Figure 10 Existing development to the west of the subject site



Source: Google street view

To the east

Land to the east of Dutton Lane consists of a mix of development sites and public domain areas. Buildings are primarily single storey and contain a mix of uses, primarily retail uses and cafes/restaurants/take-away food premises. Some office spaces (with a strong presence of medical related businesses) also occupy development to the east.

An open space pedestrian link / pedestrian mall connects Dutton Lane to Park Road to the east. This link is closed to vehicle traffic. The link provides a direct connection to Freedom Plaza.

Figure 11 Existing development to the east of the subject site



Source: Google street view

2.3.5 Surrounding road network

A Traffic Impact Assessment has been prepared by Thompson Stanbury Associates to assess the existing surrounding road network including the local road hierarchy, traffic controls at various surrounding intersections, traffic volumes and the existing intersection operation.

The site is serviced by the following roads in the surrounding area:

- » Hughes Street – a local collector function (east west)
- » Park Road – local access function (north-south) which is partly pedestrianised
- » Hill Street – local collector function (north-south)
- » John Street – local collector function (east-west)
- » Cabramatta Road West – a state road function (east-west) under the control of Roads and Maritime Services.

Dutton Lane is a one-way roadway circulating around the Council owned car parking areas (including the subject site). Access to Dutton Lane is facilitated via a single access intersection with Hughes Street, whilst egress movements from Dutton Lane are provided to two locations: one at Hughes Street and one to Hill Road.

In conjunction with providing an access function to the Council owned car parking areas, Dutton Lane provides a number of formalised loading facilities servicing the general Cabramatta Town Centre precinct.

The results of the intersection modelling and assessment of local roads is summarised below:

- » The Dutton Lane precinct currently generates between 1,000 and 1,100 peak hour vehicle trips to and from the precinct during weekday evening and weekend midday peak periods. These vehicle movements are largely accommodated at the junction of Hughes Street and Dutton Lane East, with only minor demands being accommodated at the Dutton Lane West and Dutton Lane South connections with Hughes Street and Hill Street respectively.

- » All local intersections currently operate with a good level of service during peak times providing (largely providing a level of service of B), with the exception of the junction of Hughes Street and Dutton Lane East.
- » Despite the overall operation of the junction of Hughes Street and Dutton Lane East being satisfactory, there are 'approach demands' which can result in delays being experienced by some movements, in particular motorists wishing to turn right from Dutton Lane East entering Hughes Street.
- » The junction of Hughes Street and Dutton Lane East provides a Level of Service 'E' for vehicles turning right out of the precinct.
- » The poor level of service at this junction warrants upgrading works, primarily to improve the right turn movements from Dutton Lane East which are currently serviced by an existing 'pseudo seagull' arrangement which provides for an ineffective right turn storage / acceleration lane within Hughes Street. It is also noted that westbound through traffic movements within Hughes Street are required to negotiate an undesirable lateral shift through the junction to account for the provision of exclusive turning lanes servicing Dutton Lane East within a limited pavement width.

2.3.6 Summary of Opportunities and Constraints

Based on the sites location, ownership and existing surrounding development, the following key planning and design **opportunities** have been identified and used to guide the development proposal:

- » The site is located within 200m of Cabramatta Railway Station and numerous bus services which provide convenient public transport and amenity to people who may shop or work in the vicinity;
- » The site is large in size and located in close proximity to Freedom Plaza and surrounding public domain areas, thereby providing an opportunity to extend the public domain to the Dutton Lane precinct which is currently dominated by cars;
- » The site currently only provides at-grade parking facilities; redevelopment of the site would allow for a greater diversity of uses than what is currently available;
- » The redevelopment of the site provides an opportunity to formalise the car parking arrangement and to allow for the provision of increased parking facilities, designated areas for pedestrian and vehicle movement/parking/crossing points, thereby creating a safer precinct for pedestrians and vehicles;
- » The redevelopment will allow the existing loading bay arrangement to be improved and streamlined; an increase in the number of loading bays and new associated signage would improve the delivery and loading facilities for existing and future users of the site and surrounding shops.

The key planning **constraints**, which the proposed development design has responded to, are as follows:

- » Some buildings facing the existing at-grade car park do not have active frontages and instead provide rear access to developments facing outside streets. Redevelopment of the existing car park would provide greater levels of activity in the public domain and ensure higher levels of active surveillance in the public domain.
- » The site is an infill site entirely surrounded by built form which will need to be considered in the design development.

- » There is currently a poor level of service at the junction of Hughes Street and Dutton Lane and an existing 'pseudo seagull' arrangement which provides for an ineffective right turn storage / acceleration lane within Hughes Street.
- » Dutton Lane is currently highly used for pedestrian and car movement and consequentially experiences traffic congestion. The redevelopment has the potential to improve the flow of cars through the precinct, better integrate the eastern (at-grade) and western (multi-deck) car parking areas and create a safer pedestrian environment.

3 Proposed Development

3.1 General

Pursuant to Part 4 of the Act, the Development Application seeks approval for the removal of existing ancillary structures on site, surface excavation and construction of a three storey commercial/retail development as shown in the Architectural Plans and Drawings.

Specifically, the proposal includes:

- » Removal of an existing at-grade car park and ancillary structures (including public toilet facility)
- » Construction of a commercial/retail development including car parking over three (3) levels, being:
 - > Ground level: comprises of retail floor area for 30 shops/small retail tenancies including vehicular access to the upper levels
 - > First level: comprises four commercial tenancies proposed to be used for community uses in the short term, as well as car parking accommodating 128 spaces (2 accessible)
 - > Second level: comprises of car parking accommodating 147 spaces (6 accessible)
- » Subdivision of the Council-owned lots in the Dutton Lane precinct to create seven (7) new lots
- » Modification to intersection arrangements at Dutton Lane/Hughes Street intersection.

A detailed description of the proposal is outlined below.

3.1.1 Consolidation and creation of 7 new lots

The existing lot layout consists of a mix of lots that cross the developed parts of the site and Dutton Lane. It is proposed to consolidate the existing lots and create a new lot layout as shown in Figure 12.

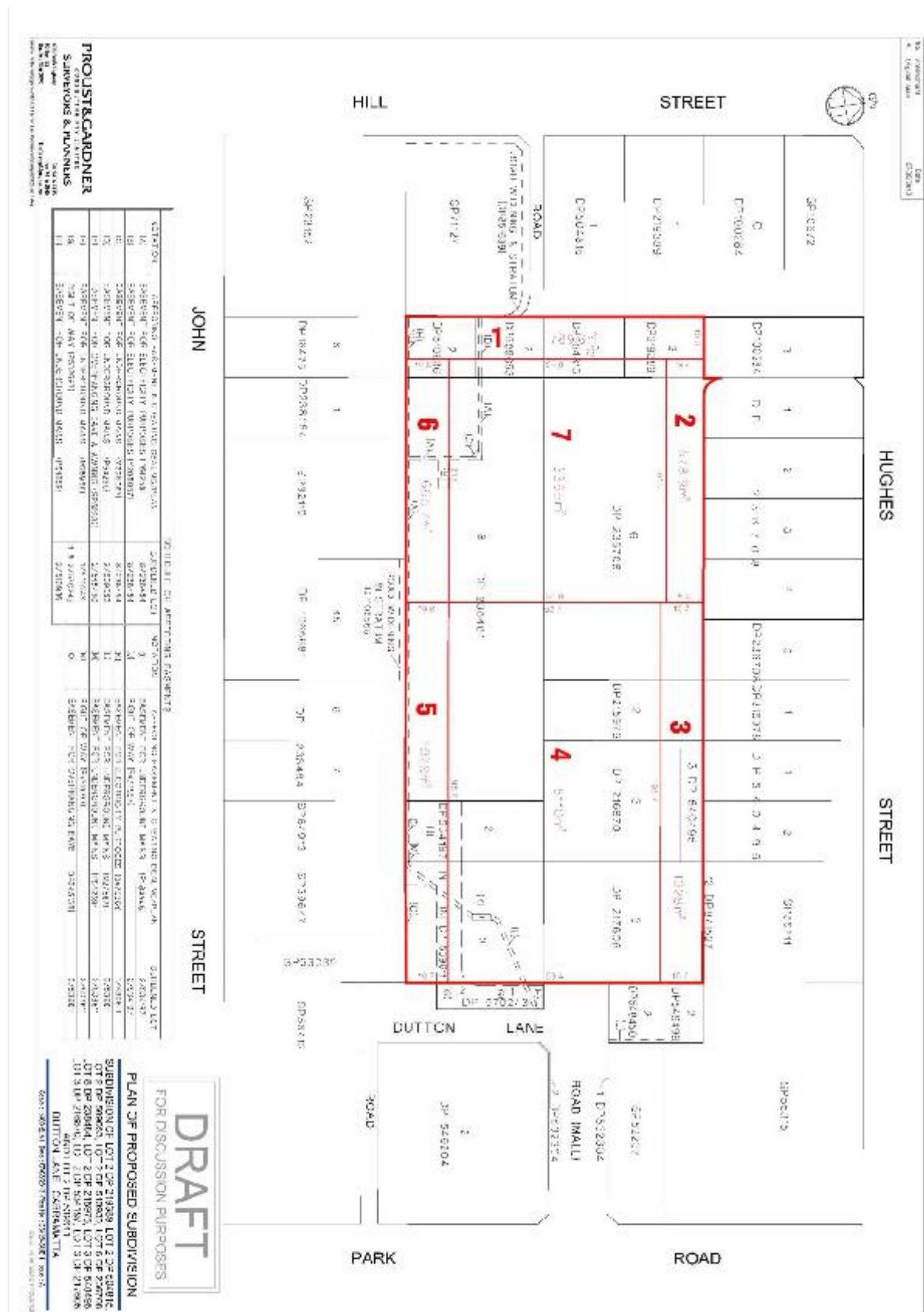
The purpose of the lot consolidation is to create a coherent lot arrangement that will enable Council to better manage the existing car parks and roadway.

The proposed lot layout will:

- » Create separate lots for the existing multi-deck car park and proposed multi-deck car park
- » Create separate lots for the Dutton Lane vehicle thoroughfare.

Refer to Plan of Proposed Subdivision prepared by Proust & Gardner Consulting in Appendix C.

Figure 12 Plan of proposed subdivision



Source: Proust & Gardner Consulting

3.1.2 Design principles

In response to the analysis of the site in Section 2 of this SEE, the key design principles for the proposed development are outlined below. Refer to Figure 13 and Figure 14 as well as the enclosed Architects Statement and the associated Architectural Plans prepared by Antoniades Architects.

Design approach

The proposal is designed to provide an excellent amenity for the community as well as a pleasant and positive contribution to the site and surrounding environment. Through focus on design excellence, the proposal would elevate Dutton Lane whilst also giving the Cabramatta Town Centre a landmark building.

The development proposes a built form that complements the existing usage of the Cabramatta Town Centre and is in keeping with the infill nature of the site. The site responds to the constraints of isolation by introducing a variety of commercial and retail spaces and public space to provide a place which people will want to visit.

Design response

The proposal allows for a dialogue between the proposed built form and its context. This provides the development with a concept plan that creates visual connectivity that would reinforce the experience and amenity of the precinct.

Viewed from across the site the development would demonstrate legibility with regards to scale and context whilst when moving through the site the homogeneity of both colour and materiality provides visual continuity and cohesion to the development.

The proposal seeks to reinforce and integrate connectivity across the site to adjoining and existing open spaces and arcades. By creating malls that respond to the existing movement patterns, and with ground floor retail enjoying a dual frontage to both street and mall, built form transparency and activation is expressed.

Linkages to Freedom Plaza through the location of mall and public open space, adds to the legibility of both movement and pedestrian access.

The design proposal incorporates a public open space that provides opportunities for passive recreation, integration with ground floor shops, and connection to Freedom Plaza.

Built form

This proposal provides a unique opportunity to create an architectural language evolved from respect for the site's built context. The architectural response has been to develop a modern and contemporary language that is contextual and identifiable. Refer to the materials and finishes for further detail.

Street awnings are proposed along the northern, eastern and southern perimeter above the footpath to protect pedestrians from the elements and make the development visually attractive to pedestrians.

Stair wells

The proposed development provides four fire-isolated stairways as per BCA requirements. These stairwells form a break in the building's façade and contribute to the visual interest of the building. The stairs are open to the street providing visibility for natural surveillance and assists as a crime prevention strategy.

Figure 13 Photomontage of the proposed development (external)



Figure 14 Photomontage of the proposed development (rear)



3.1.3 Building description

Table 3 below describes the uses, gross floor area (GFA) and building envelope data for the proposed development.

Table 3 Development data

Level	Use	GFA (sqm)	Setback / separation to adjacent buildings
Ground level	Retail	2,995	14.3m to the north
First floor	Commercial	505	12.8m to the south
	Car parking	-	2.1m to the west (car park)
Second floor	Car parking	-	19.5m to the east

Note: Fairfield LEP 2013 defines gross floor as:

The sum of the floor area of each floor of a building measured from the internal face of external walls, or from the internal face of walls separating the building from any other building, measured at a height of 1.4 metres above the floor, and includes:

(a) the area of a mezzanine, and

(b) habitable rooms in a basement or an attic, and

(c) any shop, auditorium, cinema, and the like, in a basement or attic,

but excludes:

(d) any area for common vertical circulation, such as lifts and stairs, and

(e) any basement:

(i) storage, and

(ii) vehicular access, loading areas, garbage and services, and

(f) plant rooms, lift towers and other areas used exclusively for mechanical services or ducting, and

(g) car parking to meet any requirements of the consent authority (including access to that car parking), and

(h) any space used for the loading or unloading of goods (including access to it), and

(i) terraces and balconies with outer walls less than 1.4 metres high, and

(j) voids above a floor at the level of a storey or storey above.

Facilities and internal features

The following elements are provided internal to the development:

- » Sanitary facilities (refer to Table 4 below)
- » Garbage room measuring 6.5 by 12.9 metres
- » 30 shops/retail tenancies
- » 4 commercial tenancies/offices
- » Four fire stairwells located at the north west, south west, north east and south east corners of the building
- » One lift located at the north western end of the building.

Five access points are provided to the proposed building. These access points will be controlled by security doors and roller shutters. In addition, two service corridor entries/exits are located on the southern façade of the building. These building access points will be security controlled.

It is noted that one of the entry points (at the western end of the proposed development) is located off a 2.1m wide corridor separating the existing multi-deck car park to the west from the proposed development. This access enables pedestrians to enter the development from the western end without the need to navigate Dutton Lane to enter from the east. It also provides a convenient access point to the proposed toilet facilities. Lockable security gates are proposed to be installed at the northern and southern ends of the corridor to restrict access during evenings as an additional safety measure.

Table 4 Sanitary facilities

Sanitary facilities	Required (BCA)	Required (to replace existing)	Provided / complies
Commercial			
» Male W/C	2	-	2 ✓
» Male urinal	2	-	2 ✓
» Male basin	2	-	2 ✓
» Female W/C	3	-	3 ✓
» Female basin	2	-	1 ✓
Retail			
» Male W/C	1	4	5 ✓
» Male urinal	2	4	6 ✓
» Male basin	2	2	4 ✓
» Female W/C	3	6	9 ✓
» Female basin	2	2	4 ✓

3.1.4 Public open space and landscaping

Public open space will be provided at the eastern end of the site. As shown in the Ground Level Floor Plan in Appendix B, an extensive public outdoor area is provided at the eastern end of the development adjacent to the existing pedestrian link to Park Road.

The public open space is an extension of the pedestrian footpath provided along the northern and southern perimeter of the proposed development. The paved surface treatment and landscaping design of the space aims to encourage pedestrians to visit and stay in the space.

Part of the outdoor space is covered by an awning. Other features of the outdoor space include fixed concrete seating in a decorative finish and feature colour, benches, feature lighting and a raised planted bed for plantings/shrubs. A history wall is also proposed in this area to encapsulate the rich history of Cabramatta and give the space relevance.

The outdoor space provides a total area of approximately 500 sqm of dedicated public domain. Given the proposed retail uses at the ground level, the space will be suitable to accommodate tables and chairs for outdoor dining.

Much of the seating will be protected through the use of awnings, which will enable much of the area to be used during different weather scenarios.

Pedestrian fences are provided on the outer edge of the footpaths that surround the building, providing a separation between the vehicle thoroughfare (Dutton Lane) and the pedestrian realm. Gaps in the fencing are provided where raised pedestrian crossings are proposed, thereby directing pedestrian to the safe crossing points.

Two new speed humps are proposed across Dutton Lane; at the north-eastern corner and crossing the southern stretch of Dutton Lane.

3.1.5 Access and Parking

Pedestrian access

Dutton Lane is the primary street address of the property and provides pedestrian access to the site on three frontages.

Access to Dutton Lane can be achieved via a pedestrian linkage from Parks Road / Freedom Plaza and via Hughes Road.

The proposed development seeks to formalise pedestrian crossing points across Dutton Lane through the provision of pedestrian fencing, which will direct people along the path towards 2 existing and 2 newly proposed raised marked pedestrian crossings / speed humps.

Pedestrian fencing is proposed to be provided around the building periphery to assist in the definition of the pedestrian desire lines throughout and adjoining the site and improve the existing level of safety provided within the general precinct. The fence will be strategically placed to maintain good pedestrian flow and linkages between the existing arcades to the new development.

Through site pedestrian links are proposed to be provided at ground floor level to link with existing arcades and malls, providing connectivity between the site and the general Dutton Lane precinct to Hughes Street, Park Road and John Street to the north, east and south respectively.

Car parking

Parking for the site will be provided on the first and second level of the development to ensure the ground level is maintained as an active extension of the public and private domain.

The development will provide a total of 275 spaces, 8 of which will be designed for disabled access. Considering the existing 157 spaces within the at-grade car park plus replacement of 9 on-street parking spaces from Hughes Street (refer to discussion in Loading Bays section), the proposal includes the provision of 166 spaces to replace existing spaces and a nett increase of 109 spaces.

The car parking area is proposed to be connected with two linkages to the adjoining multi-deck car parking area to the west.

It is proposed to include electronic signalling within the proposed car park as a directional tool to inform incoming vehicles of availability of car parking spaces, thereby assisting to reduce wait times and helping the flow of cars.

Internal car park circulation

Vehicle entry to the parking facility from Dutton Lane is proposed via an express access ramp at the north-western point of the site. The access ramp is proposed to provide a single ingress lane providing connectivity to an express ramp linking with the second level parking area.

Motorists will retrieve a ticket at a ticket machine and enter via a boom gate, both located at the top of the ramp on Level 2.

In addition to a primary access off Dutton Lane, a secondary access will be provided to the car park via a link on the second level of the development from the adjoining multi-deck car park to the west. Access from the newly constructed car park to the existing neighbouring car park will be available on first level of the proposed development.

The link arrangements are summarised as:

- » ingress movements to the new car parking area via a one-way (eastbound) access aisle from the existing (west) car park to the second level of new car park.
- » egress movements from the new car parking area via a one-way (westbound) access aisle from the first level of the new car park to the existing (west) car park.

Direct vehicular egress from the new car parking area to Dutton Lane is proposed via a ramp located alongside the ingress ramp. The ramp is proposed to provide a single egress lane providing connectivity from the first level parking area to Dutton Lane.

One-way connectivity between the second and first parking levels of the new car parking area is proposed via a ramp located adjoining the eastern building wall.

Vehicles will continue to access Dutton Lane via Hughes Street and will travel clockwise direction around the development.

Car spaces

The internal circulation and parking areas of the new car park have been designed to accord with the relevant requirements of AS2890.1-2004 and AS2890.6-2009.

The new parking area has been designed to accommodate standard 90 degree parking rows being servicing by one-way internal parking aisles.

Refer to Traffic Impact Assessment in Appendix D for further details on the proposed access and internal circulation arrangements and car parking bay dimensions.

Loading bays

The proposal involves changes to the loading bay arrangements in Dutton Lane. The changes serve to improve the operation of Dutton Lane by formalising existing loading bays and converting existing unsafe loading bays (i.e. 90 degree bays) to parallel bays.

The proposal also involves replacing 5 truck bays and 1 van loading space outside of Dutton Lane, on the southern side of Hughes Street (between Park Road and Dutton Lane). The resulting loss of 9 on-street public parking spaces has been incorporated into the provision of replacement car parking spaces. This arrangement is considered beneficial as:

- » A proportion of truck movements will be diverted away from Dutton Lane thereby creating a safer and more pleasant pedestrian environment
- » With convenient and direct access to shopfronts (on Hughes Street) and access to Dutton Lane via an arcade, service deliveries from the trucks to shopfronts will continue to operate efficiently and directly
- » Given the proposal to extent the existing public car parking spaces, the loss of 9 on-street car parking spaces has been offset in the provision of a new car parking facility.

One additional loading bay facility is required to service the proposed retail/commercial uses in accordance with the Cabramatta DCP. In addition, the proposal replaces all existing loading bays currently in operation.

Table 5 outlines the proposed loading bay arrangement for Dutton Lane.

Table 5 Loading bay arrangement

Existing loading bays	Required loading bays	Proposal	Complies
8 existing 90 degree bays (north-eastern corner)	8	Removed to accommodate public open space component of the development	
3 existing 90 degree bays (north-eastern corner)	3	Converted to parallel loading bay (3.5m x 18m) able to accommodate 2 MRVs	
6 existing 90 degree 'Truck Zone' bays (eastern section)	6	Converted to parallel loading bay (3.5m x 27m) able to accommodate 3 MRVs	
3 existing 3.5m wide parallel bays (south-eastern corner)	3	Extended by 12m thereby able to accommodate and additional 2 vans (5 vans in total)	
6 existing 3.5m wide parallel bays (south-eastern/central)	6	Reduced by 6m to accommodate new raised pedestrian platform crossing over Dutton Lane, thereby reducing capacity to 5 vans	
-	1 loading bay (3.5m x 9m) to service the proposed development	2 loading bay areas are proposed within the footprint of the proposed development site, to the north-east and south-east of the building respectively. The bays will provide capacity for 6 vans Convert existing time limited car parking located on the southern side of Hughes Street (between Park Road and Dutton Lane) to a sign posted loading zone 55m in length, thereby able to accommodate 1 van loading space (6m) and 5 MRVs (45m)	
26 loading bays	27 loading bays	27 loading bays (10 truck/MRV bays and 17 van loading spaces)	✓

Intersection treatment

The Traffic Impact Assessment prepared by Thompson Stanbury Associates assessed the existing surrounding road network including the local road hierarchy, traffic controls at various surrounding intersections, traffic volumes and the existing intersection operation.

Based on the Traffic Impact Assessment, an improvement to the junction of Hughes Street and Dutton Lane East intersection is proposed. A formalised 'seagull' arrangement is proposed, combining the exclusive left and through travel lanes within Hughes Street to the east of the junction to allow for the creation of a formal and exclusive right turn acceleration lane within Hughes Street for vehicles turning right out of Dutton Lane East.

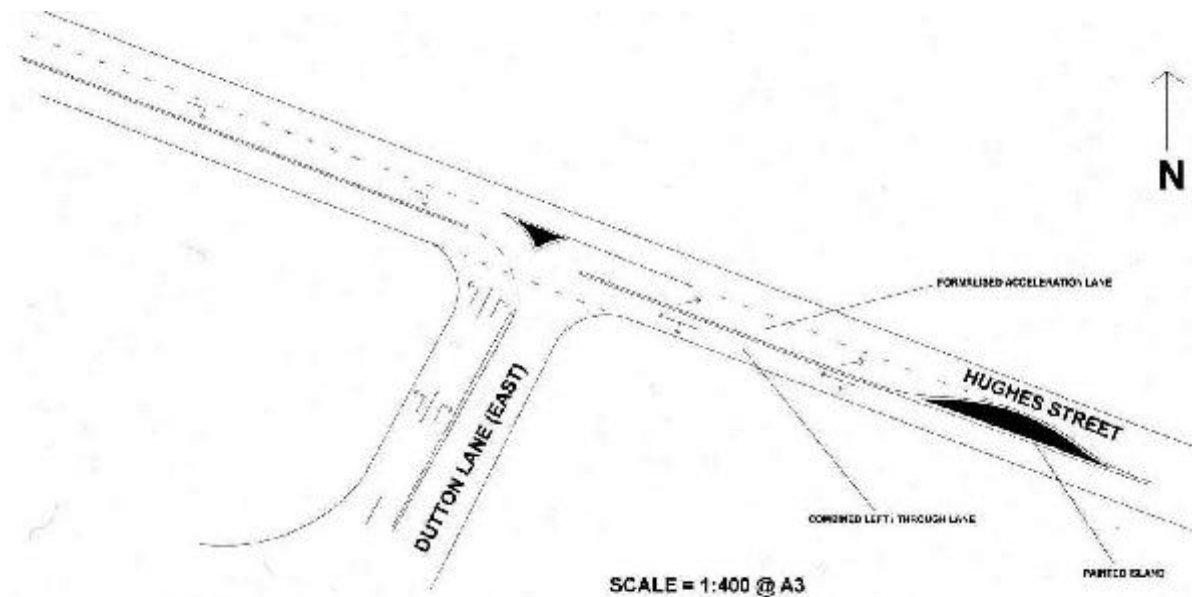
No major civil works are required to accommodate this junction modification and there will be no loss in on-street parking.

Refer to proposed intersection treatment arrangement in Figure 15.

The alterations include:

- » a formalised acceleration lane for vehicles exiting Dutton Lane (right) to Hughes Street
- » a combines left/through lane on Hughes Street (between Dutton Lane and Park Road) travelling in a westbound direction
- » introduction of a painted island in Hughes Street (between Dutton Lane and Park Road).

Figure 15 Proposed intersection arrangement (Hughes Street and Dutton Lane)



Source: Thompson Stanbury Associates

3.1.6 Materials and finishes

The overall design concept provides a contemporary and strong architectural expression, which reflects and sits well within its context and surrounds. The development also fits well to the proposed scale and desired character of the precinct.

The façade of the building has been designed ensure articulation and visual interest through the provision of perforated aluminium panels, which also disguise the car parking levels. This treatment breaks up the bulk of the building and makes the building visually interesting rather than a predominance of large flat walls.

Glazing is provided along the north, east and west facades of the building at ground level to allow for strong visual connections between private and public spaces and increased activation of the pedestrian realm. This treatment enhances the visual character of the building, as well as provides active surveillance and safety for pedestrians moving around the site.

Awnings are provided along the northern, eastern and southern façades of the building to protect pedestrians from the elements and to enhance the visual appearance of the building. The awnings will be designed in such a way that they can be individually lowered when required, to act as a security shutter for shop fronts during closure times.

These shutters will provide a functional and visually pleasant solution to roller shutters which primarily characterise retail developments.

The following details are extracted from the Architectural Statement provided by Antoniades Architects:

Materiality

The colours and materials have been carefully selected to complement both the architectural expression as well as its surroundings. The colours chosen are shades of natural palettes, that being an earthy red and grey, as well as the use of natural off form concrete, which provide a soft yet bold identity to the building, allowing it to act as a landmark within its context.

Elevation

The composition of the proposed facade is primarily made up of aluminium panels and off form concrete walls. The aluminium panels are perforated with a regulated pattern to create a visual interest across the facade. The materials have been selected for their robustness and durability which are desirable characteristic for a public building of this nature.

The proposed colours to be used in the facade are in tones of earthy red to provide a softer and natural nuance to the carpark, whilst the concrete will remain in its natural colour with a honed finish.

Facade Design Detail

The facade system contains two key elements – off form concrete walls and perforated aluminium panelling fixed to steel framing. This system has been designed so that most prefabrication are done off-site to reduce on-site welding and fabrication which would improve construction efficiency significantly. The façade has also been designed to comply with the specific requirements of the BCA of providing fall protection as well as eliminating any foot-hold so that it can not be climbed.

The aluminium panel have patterned perforation to provide right amount of balance of screening to the car park as well as visual permeability to provide security, almost acting like a veil to the building. The perforated panels are deconstructed around the commercial space to enhance visual interest as well as allowing stronger transparency and visual connection from the commercial tenancy.

Entrances to the retail on ground level are signified by expressing the screen further in a way such that the screen becomes deeper in profile. This design of panels folding at entrances also provides opportunity for additional lighting to penetrate through from the car park.

Lighting

Energy efficient fittings are proposed throughout the car park and used in conjunction with photocell and time switches to reduce energy consumption. The external roof level will be fitted with unobtrusive lighting to prevent light spilling onto the adjacent properties.

All lightings are to meet Australian Standard for car parks.

3.1.7 Waste Management

A garbage room will be provided on the ground floor of the proposed development, towards the south eastern corner of the building. The garbage room has dimensions of 6.5m x 12.9m.

The garbage room will provide space for waste management and storage associated with the operation of retail and commercial developments on the ground and first floor. The room will be accessible through a door via a service corridor which provides direct secure access to the southern side of the proposed building where garbage pick-up is proposed.

3.1.8 Plant

A plant room containing the buildings mechanical equipment is proposed to be located on the roof of the building (Level 2) approximately in the centre of the floor plate. The plant room dimensions are 10 metres x 5.2 metres (x 2.6 metres in height). Screening is provided to the sides of the plant room to improve its visual appearance.

3.1.9 Stormwater Management

Stormwater will be conveyed from the site to covered stormwater pits located around the periphery of the proposed building. Some existing stormwater pits will be used for stormwater drainage purposes while redundant pits and pipework will be capped off and removed. Refer to Stormwater drainage plan prepared by itmDesigns in Appendix G for details.

3.1.10 Relocation of substation

An existing substation is located generally in the south-eastern portion of the site. It is proposed to relocate the existing substation to the eastern side of Dutton Lane at the south-eastern corner of the site (Refer to Ground Floor Level Plan prepared by Antoniades Architects).

Enquiries are currently underway with Endeavour Energy to seek its consent to relocate the existing substation. There may also be a requirement to install an additional substation to service the development and such is being assessed by Endeavour Energy and council's consultant Electrical Engineer.

3.1.11 BCA

An assessment of the proposal has been undertaken against the Deemed-to-Satisfy provisions of the relevant sections of the Building Code of Australia (BCA) 2013 by Steve Watson & Partners (refer to Appendix H). For the purposes of the BCA Assessment the building has been designated as follows:

- » Ground Floor (retail) – class 6

- » Level 1 (office and car park) – class 5 and 7a
- » Level 2 (car park) – class 7a.

The BCA Assessment revealed three issues of non-compliance with the Deemed-to-Satisfy provision, however an alternative solution has been proposed for each issue which is found to satisfy BCA performance requirements.

The three alternative solutions proposed:

- » The travel distance between alternative exits measured through the point of choice on the ground floor is up to 66m from the exit adjacent to shop 25 to the exit adjacent to shop 4. The BCA provision is 60m. However, this arrangement is deemed to satisfy performance requirements DP4 and EP2.2 and therefore is considered appropriate.
- » Egress widths on the ground floor are found to be non-compliant, however the alternative solution proposed is found to satisfy performance requirements DP4 and EP2.2 and therefore is considered appropriate.
- » The horizontal exits in the fire wall separating the first floor office from car park comprise of more than half the required exits. However, this arrangement is deemed to satisfy performance requirements DP4 and EP2.2 and therefore is considered appropriate.

A number of other matters have been highlighted as issues to be addressed in the Construction Certificate stage. The proposal is found to satisfy the BCA with alternative solutions for three issues. Refer to BCA Assessment in Appendix H for further details.

4 Statutory context

This section of the SEE addressees the statutory planning controls and policies relevant to the proposed development.

4.1 Environmental Planning & Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* (EP&A Act) institutes the system of environmental planning and assessment in NSW. Under Part 4 (Clause 76A) consent is required for the purpose of carrying out the proposed development.

Pursuant to Section 79C(1)(a) of the Act the consent authority is required to consider (where relevant to the proposal) the provisions of:

- i. any environmental planning instruments, and*
- ii. any draft environmental planning instrument that is or has been placed on public exhibition, and*
- iii. any development control plan, and*
- iv. the regulations.*

The next section provides an overview of planning instruments and controls relevant to the proposed development.

4.1.1 State Environmental Planning Policies

State Environmental Planning Policies (SEPP's) address issues significant to the state. A review of current SEPPs has determined that SEPP (infrastructure) 2007 is relevant to the proposal.

SEPP (Infrastructure) 2007

SEPP (Infrastructure) 2007 provides a consistent planning regime for infrastructure and the provision of services across NSW, along with providing for consultation with relevant public authorities during the assessment process. It includes provisions relating to traffic generating development which are defined in Schedule 3 of the SEPP.

Schedule 3 of SEPP (Infrastructure) 2007 sets out the types of traffic generating development that are to be referred to the Roads and Maritime Services (RMS).

For the proposed uses, SEPP (Infrastructure) 2007 includes the following thresholds:

Table 6 SEPP (Infrastructure) traffic generating development thresholds

Purpose of development	Size or capacity—site with access to any road	Size or capacity—site with access to classified road or to road that connects to classified road (within 90m(<i>(Not relevant to the development)</i>)	Applicability to the proposed development
Subdivision of land	200 or more allotments where the subdivision includes the opening of a public road	50 or more allotments	Not applicable
Area used exclusively for parking	200 or more motor vehicles	50 or more motor vehicles	Yes
Commercial premises	10,000 sqm in area	2,500 sqm in area	Not applicable
Shops	2,000 sqm	500 sqm	Yes

The proposal triggers the minimum thresholds for traffic generating development as it is proposing to formalise a car parking facility that provides 157 spaces with a new facility accommodating 275 spaces.

Clause 104 of SEPP (Infrastructure) 2007 provides:

Before determining a development application for development to which this clause applies, the consent authority must:

- (a) give written notice of the application to the RTA (now RMS) within 7 days after the application is made, and*
- (b) take into consideration:*
 - (i) any submission that the RTA provides in response to that notice within 21 days after the notice was given (unless, before the 21 days have passed, the RTA advises that it will not be making a submission), and*
 - (ii) the accessibility of the site concerned, including:*
 - (A) the efficiency of movement of people and freight to and from the site and the extent of multi-purpose trips, and*
 - (B) the potential to minimise the need for travel by car and to maximise movement of freight in containers or bulk freight by rail, and*
 - (iii) any potential traffic safety, road congestion or parking implications of the development.*

The Traffic Impact Assessment has been prepared by Thompson Stanbury Associates to examine the transport, traffic and safety implications from the proposed development.

It summarises that the proposal will result in improved traffic and safety arrangements for the surrounding street network and the Dutton Lane precinct.

Key findings of the Assessment are summarised below:

- » The **proposed intersection improvements** to the Hughes Street / Dutton Lane junction (described in Section 3.1.5 of this SEE) will improve the current operational performance and

safety of the intersection, as well assist in the ability of the intersection to accommodate additional traffic projected to be generated by the subject development.

- » The altered traffic management arrangement at the junction of Hughes Street and Dutton Lane East can be accommodated without any impact on existing on-street parking supply within Hughes Street.
- » The **proposed pedestrian improvements** to Dutton Lane (i.e. introduction of formalised raised marked pedestrian crossing points over Dutton Lane) will provide more formal pedestrian movements in conjunction with the pedestrian fencing that will direct pedestrians to safer desire lines for crossing Dutton Lane. This will significantly enhance existing pedestrian movements which currently occur throughout the precinct in an ad-hoc manner, with only informal connections between the central parking area and the periphery shopping malls.
- » The **proposed loading bay arrangements** will significantly improve the safety and operation of loading and servicing of shops in the Dutton Lane precinct. Currently, the lack of governance of the formalised and sign posting loading bays within periphery Dutton Lane roadway result in a significant misuse of the bays, whereby angled parking is provided within loading areas which are intended to accommodate parallel parking and the loading bays are occupied regularly by vehicles not authorised or intending to undertake loading / unloading activities. This results in significant undesirable conflict between the Park Road pedestrian mall and the reversing manoeuvres associated with the 90 degree angled loading bays within the eastern portion of the precinct.
- » The **proposed design, ingress/egress and circulation arrangements** will provide for significantly improved precinct accessibility, by improving the Hughes Street / Dutton Lane East junction and providing an additional multi-storey car park with express access and increased queuing capacity.

Based on modelling undertaken in the Traffic Impact Assessment:

- » The proposed development is projected to generate 130 and 250 peak hour vehicle trips to and from the precinct during weekday evenings and weekend peak periods. This represents approximately 10% and 20% additional traffic movements to and from the precinct during weekday evening and weekend midday peak periods respectively.
- » Incorporating the recommended formalised 'seagull' arrangement and the additional traffic projected to be generated by the subject development, the level of service provided at the junction of Hughes Street and Dutton Lane (East) is projected to improve from 'E' to 'C', representing satisfactory conditions.
- » All other surrounding intersections are capable of accommodating the additional traffic projected to be generated by the subject development without infrastructure improvements / alterations.

The Traffic Impact Assessment concludes that there are no traffic related issues associated with the proposed development which would prevent approval of the proposed development. Refer to Traffic Impact Assessment in Appendix D for further details.

4.1.2 Fairfield Local Environmental Plan 2013

Fairfield Local Environmental Plan 2013 (FLEP) is the key local environmental planning instrument governing land use in the Fairfield LGA. The following sections address the relevant sections of FLEP and the proposals compliance with relevant controls therein.

Zoning and permissibility

The site is located within a B4 Mixed Use Zone.

The objectives of the B4 Mixed Use Zone are:

- » *To provide a mixture of compatible land uses;*
- » *To integrate suitable business, office, residential, retail and other development in accessible locations so as to maximise public transport patronage and encourage walking and cycling;*
- » *To support the development of Prairiewood, Fairfield and Cabramatta as the principal locations for specialist cultural, retail, business, tourist and entertainment facilities and services.*

Refer to Figure 16.

The following summarises the types of uses that are permitted and prohibited in the B4 zone:

2 Permitted without consent

Environmental protection works; Home-based child care; Home occupations

3 Permitted with consent

*Boarding houses; Child care centres; **Commercial premises**; Community facilities; Educational establishments; Entertainment facilities; Function centres; Hotel or motel accommodation; Information and education facilities; Medical centres; Passenger transport facilities; Recreation facilities (indoor); Registered clubs; Respite day care centres; Restricted premises; Roads; Seniors housing; Shop top housing; **Any other development not specified in item 2 or 4***

4 Prohibited

Agriculture; Air transport facilities; Airstrips; Animal boarding or training establishments; Attached dwellings; Biosolids treatment facilities; Boat building and repair facilities; Boat launching ramps; Boat sheds; Camping grounds; Caravan parks; Cemeteries; Charter and tourism boating facilities; Crematoria; Depots; Dual occupancies; Dwelling houses; Eco-tourist facilities; Environmental facilities; Exhibition villages; Extractive industries; Farm buildings; Farm stay accommodation; Forestry; Freight transport facilities; Heavy industrial storage establishments; Helipads; Home businesses; Home industries; Home occupations (sex services); Industrial retail outlets; Industrial training facilities; Industries; Jetties; Marinas; Mooring pens; Moorings; Mortuaries; Multi dwelling housing; Open cut mining; Recreation facilities (major); Research stations; Resource recovery facilities; Rural industries; Rural workers' dwellings; Secondary dwellings; Semi-detached dwellings; Sewage treatment plants; Sex services premises; Storage premises; Transport depots; Truck depots; Vehicle body repair workshops; Warehouse or distribution centres; Waste disposal facilities; Water recreation structures; Water recycling facilities; Water supply systems; Wharf or boating facilities; Wholesale supplies

The proposed development incorporates commercial uses (which include retail uses) and a car park. These uses are defined by FLEP as follows:

Table 7 Fairfield LEP Land Use definitions

Term	FLEP definition	Permissible in B4 zone
Car park	Car park means a building or place primarily used for the purpose of parking motor vehicles, including any manoeuvring space and access thereto, whether operated for gain or not.	Permissible – as 'Any other development not specified in item 2 or 4'
Commercial premises	Commercial premises means any of the following: (a) business premises (b) office premises (c) retail premises.	Permissible
Retail premises	Retail premises means a building or place used for the purpose of selling items by retail, or hiring or displaying items for the purpose of selling them or hiring them out, whether the items are goods or materials (or whether also sold by wholesale), and includes any of the following: (a) bulky goods premises (b) cellar door premises (c) food and drink premises (d) garden centres (e) hardware and building supplies (f) kiosks (g) landscaping material supplies (h) markets (i) plant nurseries (j) roadside stalls (k) rural supplies (l) shops (m) timber yards (n) vehicle sales or hire premises but does not include highway service centres, service stations, industrial retail outlets or restricted premises.	Permissible – under the group term commercial premises.

Comment:

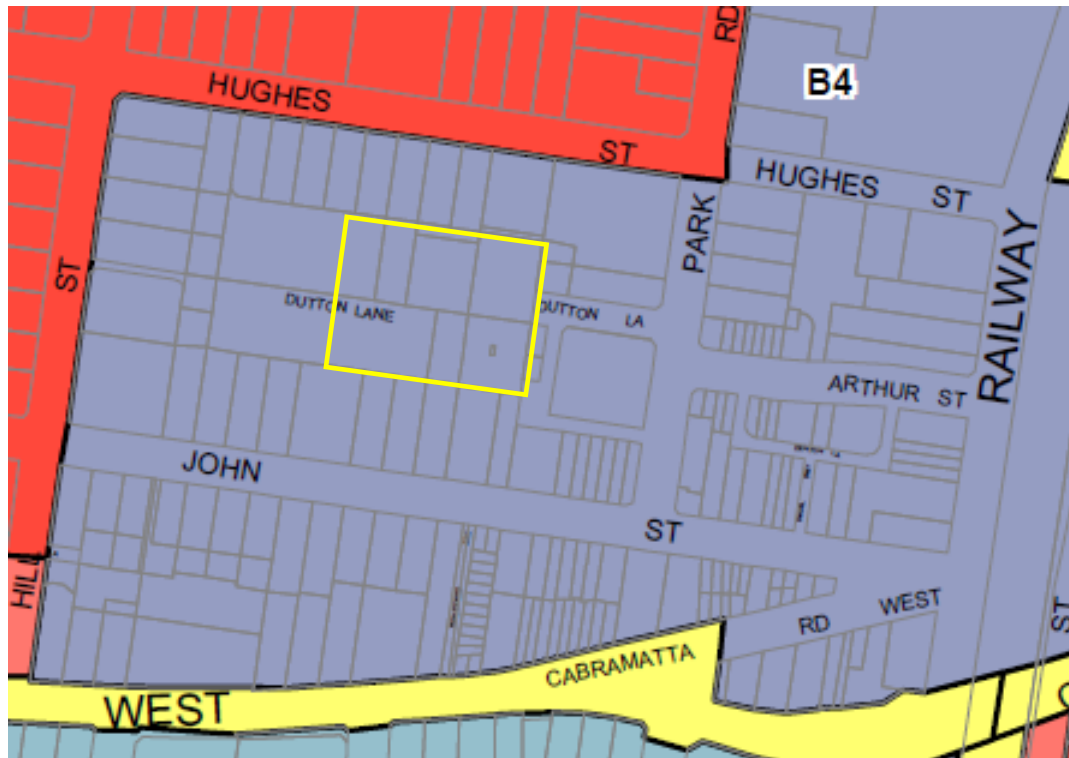
The proposed development is permissible within the B4 Mixed Use Zone and is consistent with the zone objectives for the following reasons:

- » The proposed development proposes a mix of retail and commercial office facilities within a single building. Car parking proposed in the upper half of the development will ensure access for workers and visitors of the new development, and will provide parking for people visiting Cabramatta Town Centre.
- » The proposed development provides a mix of business/office and retail facilities in an accessible location, being within 200 metres of Cabramatta Railway Station and regular bus services, as

well as residential development surrounding the Town Centre which encourages people to walk and cycle. Parking on the site ensures accessibility is considered for people with a disability and people not otherwise within walking/cycling distance.

- » The proposed development supports the development of Cabramatta as a principal location for specialist cultural, retail, business, tourist and entertainment facilities, as it provides new retail and commercial floor area on the ground and first level that can be utilised for the purposes of cultural, retail, business, tourist and entertainment facilities and services and to complement the surrounding uses.

Figure 16 Zoning Map



Source: Thompson Stanbury Associates

Height of buildings

FLEP defines building height as the "vertical distance between ground level (existing) and the highest point of the building, including plant and lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like".

Clause 4.3 establishes that the maximum permissible building height for the site is 16 metres.

The objectives of this clause are as follows:

- » To establish the maximum height for buildings
- » To ensure that the height of buildings complements the streetscape and character of the area in which the buildings are located
- » To minimise the visual impact, disruption of views, loss of privacy and loss of solar access to existing development.

However, clause 7.3 provides that the height of a building on land identified as "Cabramatta—Area A" on the Town Centre Precinct Map must not exceed 10 metres unless at least 50% of the site area will be landscaped open space.

Therefore, the permissible height is taken to be 10 metres for the subject site given that the proposal is to replace an existing at-grade car parking facility where zero landscaping is provided with a new car parking facility. However, it is noted that the proposal has been designed to replace all existing car parking spaces as required under the Cabramatta DCP, whilst activating the pedestrian realm and providing new, useable public open space in the form of a new public plaza area (extension of the surrounding pedestrian Plazas in Cabramatta Town Centre). The public domain component of the proposal is designed to provide good levels of pedestrian amenity and activity, thereby achieving related objectives such as safety and Crime Prevention Through Environmental Design (CPTED).

The maximum building height are shown on the Section and Elevation drawings and described as:

- » Maximum height to top of plant room (RL 29.40): **9.8 metres**
- » Maximum height to top of fire stairs (RL 30.45): **10.85 metres**
- » Maximum height to canopy over lift lobby (RL 30.45): **10.10 metres**
- » Maximum height to top of facade wall (not including balustrade): **7.2 metres**

Note: Ground Level is measured at RL 19.60.

The proposal is therefore non-compliant with the 10 metres height limit. However, the non-compliance is limited to four points within the proposed development where the fire stairs/lift lobby are located. The location of four fire stairwells generally at the four corners of the proposed development is a necessary component of the building to satisfy the BCA.

Despite the non-compliance of 0.85m at the top of the four stairwells and 0.10m at the lift lobby canopy, the proposal still achieves the objectives of clause 4.3.

This SEE contains a request to consider a variation to the building height standard make under clause 4.6 of the FLEP. Refer further below in this section of the SEE for justification for the variation to the building height standard against clause 4.6 of FLEP.

Floor Space Ratio

The maximum permissible floor space ratio (FSR) for the site is 2.5:1.

FLEP defines FSR as is *the ratio of the gross floor area of all buildings within the site to the site area.*

The following definition of GFA applies:

Gross floor area means the sum of the floor area of each floor of a building measured from the internal face of external walls, or from the internal face of walls separating the building from any other building, measured at a height of 1.4 metres above the floor, and includes:

- (a) the area of a mezzanine, and*
- (b) habitable rooms in a basement or an attic, and*
- (c) any shop, auditorium, cinema, and the like, in a basement or attic,*

but excludes:

- (d) any area for common vertical circulation, such as lifts and stairs, and*
- (e) any basement:*
 - (i) storage, and*
 - (ii) vehicular access, loading areas, garbage and services, and*
- (f) plant rooms, lift towers and other areas used exclusively for mechanical services or ducting, and*
- (g) car parking to meet any requirements of the consent authority (including access to that car parking), and*

- (h) any space used for the loading or unloading of goods (including access to it), and
- (i) terraces and balconies with outer walls less than 1.4 metres high, and
- (j) voids above a floor at the level of a storey or storey above

The proposed development complies with the FSR control as it has a FSR of 0.94:1. This FSR has been calculated taking into account 4,130sqm of GFA on the Ground Floor and 660sqm of GFA on the First Floor. As shown in the Plan of proposed subdivision (refer to Figure 12 of this SEE), the site area of the proposed development site (building footprint) is 5,110sqm.

Heritage

The site is not a heritage item and is not located on land within a heritage conservation area.

The site is located approximately 75 metres from a listed heritage item (the Pai Lau Gateway). Refer to Figure 17 and Figure 18.

Figure 17 Freedom Plaza Pai Lau Gateway Heritage item



Source: Nearmaps

Figure 18 Pai Lau Gateway



Source: Fairfield City Council

The proposed development will not create an obstruction to views or access to the Pai Lau Gateway. Advice from Council's Heritage Branch regarding the nearby heritage item is that a heritage impact statement will not be required as the proposed development does not represent a potential impact on the heritage value of the item.

Furthermore, the proposal to extend the public outdoor space/public realm to the Dutton Lane precinct will improve the amenity and pedestrian experience of the wider Town Centre area. In this regard, an increase in pedestrian activity in and around the precinct will allow greater visitorship to the area and thereby potentially greater numbers of people to visit the heritage item.

Essential services

Clause 6.9 of FLEP states that development consent must not be granted to development unless the consent authority is satisfied that any of the following services that are essential for the development are available (or that adequate arrangements have been made to make them available) when required:

- » the supply of water
- » the supply of electricity
- » the disposal and management of sewage
- » stormwater drainage or on-site conservation
- » suitable vehicular access.

All essential services will be available to the proposed development. In particular, appropriate vehicle access to the proposed car park has been provided (refer to discussion in section 4.1.1 and 3.1.5 of this SEE).

Request for Clause 4.6 Variation

Clause 4.6 of FLEP aims to provide flexibility in the application of certain development standards. This section of the SEE requests the consent authority consider the proposal to vary the height of buildings control by a maximum of 0.85 metres.

The objectives of Clause 4.6 are:

- » *To provide an appropriate degree of flexibility in applying certain development standards to particular development,*
- » *To achieve better outcomes for and from development by allowing flexibility in particular circumstances.*

Clause 4.6(2) provides that:

Development consent may, subject to this clause, be granted for development even though the development would contravene a development standard imposed by this or any other environmental planning instrument.

Clause 4.6(3) outlines the circumstances in which development that contravenes a development standard can be approved. It states:

Development consent must not be granted for development that contravenes a development standard unless the consent authority has considered a written request from the applicant that seeks to justify the contravention of the development standard by demonstrating:

- (a) that compliance with the development standard is unreasonable or unnecessary in the circumstances of the case, and*
- (b) that there are sufficient environmental planning grounds to justify contravening the development standard.*

Under Clause 4.6(4), the following is required in order to grant consent to development that contravenes a development standard:

Development consent must not be granted for development that contravenes a development standard unless:

(a) the consent authority is satisfied that:

(i) the applicant's written request has adequately addressed the matters required to be demonstrated by subclause (3), and

(ii) the proposed development will be in the public interest because it is consistent with the objectives of the particular standard and the objectives for development within the zone in which the development is proposed to be carried out, and

(b) the concurrence of the Director-General has been obtained.

Under Clause 4.6(5), in deciding whether to grant concurrence, the Director-General must consider:

- » *whether contravention of the development standard raises any matter of significance for State or regional environmental planning, and*
- » *the public benefit of maintaining the development standard, and*
- » *any other matters required to be taken into consideration by the Director-General before granting concurrence.*

The following describes and justifies the proposed variation to building height.

Request

This request is made pursuant to Clause 4.6 of FLEP to allow the proposed development to exceed the maximum height limit of 10 metres by a maximum of 0.85 metres at four isolated points in the proposed building, being at the top of the fire stairwell. The encroachment over 10 metres in height occurs on the north western, north eastern, south eastern and south western corners of the development where the proposed fire stairs are located. At the north-western corner, a canopy over the lift lobby also exceeds the 10 metres height limit by 0.10 metre.

The dimension of the fire stairwells is 3.3 metres x 5.0 metres (approximately). Given the length of the proposed building is approximately 89 metres (east to west), the portion of the northern and southern facades that exceeds the 10 metre height limit represents only 3.7% of the total facade (per stairwell).

Figure 19 illustrates the variation on the north-western corner of the building.

Compliance with the development standard is unreasonable or unnecessary in the circumstances of the case

This purpose for seeking the variation is primarily due to the provision of four fire-isolated stairways as per Building Code of Australia (BCA) requirements. Considering the requirement to comply with BCA standards, the requirement to comply with the development standard is unnecessary in this case.

The fire stairs are located to ensure the entire building has a high level of accessibility during the event of an emergency in which lifts are unavailable or unsafe.

It is noted that the components of the building which exceed the height limit (fire-isolated stairwell and loft lobby) are related to the provision of access to the rooftop and public car parking. These components are required to ensure safe and efficient access for staff and visitors to the site. No part of the proposal associated with the provision of commercial/retail floor space exceeds the height limit.

The majority of the remaining component of the building falls within the allocated height limit. Given that the height overrun does not provide any spatial or financial advantage to retail and commercial tenants of the development (i.e. through the provision of increased floor area or the like), the request for a variation is deemed reasonable.

There are sufficient environmental planning grounds to justify contravening the development standard

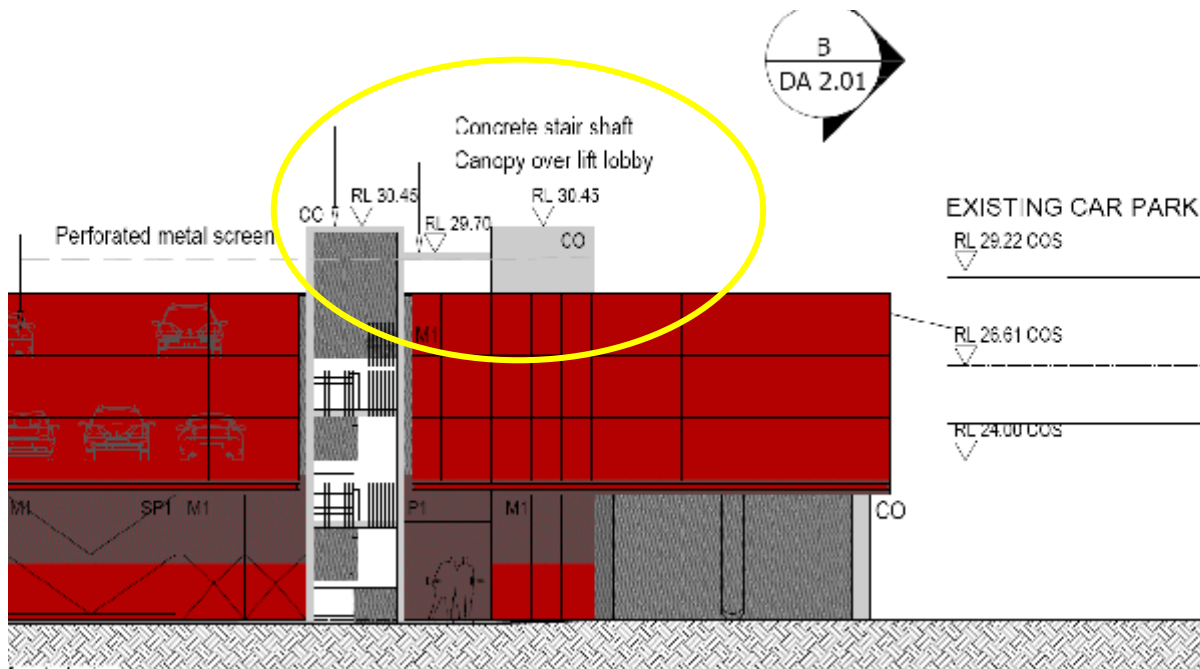
Given the minor nature of the non-compliance (representing approximately 7.4% of the total facade on the northern and southern sides of the building only), the variation will not result in any adverse amenity impacts on the surrounding area.

The stairwells have been specifically designed to form a break in the building façade and contribute to the visual interest of the building. As shown in the Elevation Drawings prepared by Antoniades Architects, the stairwells are open to the street, providing visibility from within the stairwell to the public domain for natural surveillance. This assists as part of the crime prevention strategy. It also ensures people accessing the stairwells can be seen (and heard) from the surrounding area.

In terms of design, the open nature of the stairwells means that the structures do not present as a bulky element to the building, but rather have been designed to add interest and break up with facades.

Considered on merit, the proposed variation is found to be consistent with Clause 4.6(3) and therefore worthy of support.

Figure 19 Proposed building height variation (north western corner)



Source: Antoniades Architects

4.1.3 Draft planning instruments

None applicable.

4.1.4 Cabramatta Development Control Plan 2013

Whilst not a statutory document, it is acknowledged that the site is affected by Cabramatta Development Control Plan 2013 (CDCP).

CDCP is a non-statutory document which supplements the local environmental plan by providing guidance to a consent authority and to people who are proposing to undertake development on land to which the DCP applies.

On 1 March 2013, *Environmental Planning and Assessment Amendment Act 2012* (Amendment No. 93) was introduced. The Amendment outlined the purpose, status and content of DCPs, and how they are to be taken into account during the development assessment process.

NSW Department of Planning and Infrastructure Planning Circular PS 13-003 *Proclamation of certain provisions of the EP&A Amendment Act 2012*, explains the changes as follows:

"The Amendment Act reinforces that the provisions contained in a DCP are not statutory requirements and are for guidance purposes only.

The Amendment Act introduces flexibility in the way in which provisions in a DCP are to be considered by a consent authority when assessing a development application. Consent authorities now have more power to be flexible and consider innovative solutions when assessing development proposals, helping to create good planning outcomes.

Similarly, if a development application does not comply with provisions in a DCP, a consent authority must be flexible in the way it applies the controls and also allow for reasonable alternative solutions to achieve the objectives of those standards."

The site is located in Precinct 2 of DCP. Generally the proposal complies with the objectives and performance criteria for Precinct 2. A compliance table detailing how the proposal responds to the guidelines and objectives in CDCP is provided in Appendix A.

5 Assessment of planning issues

This section of the SEE provides an assessment of the proposal against relevant matters for consideration under section 79C(1) of the EP&A Act.

Section 79C(1) states:

In determining a development application, a consent authority is to take into consideration such of the following matters as are of relevance to the development the subject of the development application:

(a) the provisions of:

- (i) any environmental planning instrument, and*
- (ii) any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Director-General has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved), and*
- (iii) any development control plan, and*
- (iiia) any planning agreement that has been entered into under section 93F, or any draft planning agreement that a developer has offered to enter into under section 93F, and*
- (iv) the regulations (to the extent that they prescribe matters for the purposes of this paragraph), and*
- (v) any coastal zone management plan (within the meaning of the Coastal Protection Act 1979),*

that apply to the land to which the development application relates,

(b) the likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality,

(c) the suitability of the site for the development,

(d) any submissions made in accordance with this Act or the regulations,

(e) the public interest.

It is noted that Section 4 of this SEE has addressed section 79C(1)(a) of the EP&A Act. Therefore, this section addressed subclauses (b), (c), (d) and (e) of the EP&A Act.

5.1 Assessment of natural and built form impacts

5.1.1 Natural environment

The proposed development is not located within an ecologically sensitive environment given its significant existing built form and location within a well-established town centre.

The proposal involves the removal of an existing at-grade car park. Within the location of the existing at-grade car park there is no landscaping, open space or water bodies that will be negatively impacted to allow for the construction of the proposed development.

It is proposed that during the construction phase of the proposed development, appropriate measures will be employed to ensure that any potential negative impacts associated with stormwater or sediment run-off are appropriately managed.

It is noted that measures will be incorporated into the design of the development to ensure appropriate connection points to Council's existing stormwater system are in place.

5.1.2 Built form

Built form, scale and visual impact

As shown in the Architectural Plans, the proposed envelope is of an appropriate built form based on the analysis of the site (refer to Section 2 of this SEE) and the Urban Design Principles provided at Section 3.

From the east, the development provides a visual barrier to the large multi-deck car-park located to the west of the proposed development.

The proposal is slightly lower than the neighboring car park, which results in a minor stepping down of built form towards Cabramatta Town Centre (east of the development). However, the existing multi-deck car park will be predominantly screened from view with a new, architecturally designed building that will improve the visual amenity of the site.

Awnings, windows and other openings along the northern, eastern and southern facades of the building (discussed in detail in Section 3 of this SEE) improve the overall appearance of the building in terms of its bulk.

Visual interest is created through the use of outer facade treatments (perforated aluminum panels) as well as modulation in the facade through open fire stairs at each corner of the building. This ensures the car park element of the building is appropriately screened, thereby avoiding unsightly car parking elements.

Public domain

The proposed development enhances and improves the streetscape and public domain of the wider Dutton Lane precinct. Additional public domain that connects to the core of Cabramatta's existing retail core is a major element of the proposal.

The development proposes to remove an older at-grade car park, and old toilet blocks. The replacement building and associated public outdoor space provide a public benefit and the general standard of Dutton Lane for pedestrians will be significantly improved. The proposed development will:

- » Improve existing pedestrian connections by formalising pedestrian routes that link the development to existing shopping malls and arcades
- » Install fencing along footpaths to direct people towards raised marked pedestrian crossings lane, thereby minimising pedestrian / vehicle conflict
- » Remove the existing unsafe 90 degree angled loading bays around high pedestrian areas
- » provide internal arcades and ingress points at ground level which provide good connections to existing arcades and malls.

Retail activity at the ground level will provide a point of attraction for pedestrians. The proposal includes the provision of public open space at the eastern end of the site. This will provide a link to Freedom Plaza. This space will include public seating, landscaping and lighting as well as designed concrete flooring / paving to provide a visually interesting addition to the Cabramatta public domain. A history wall about Cabramatta is also proposed to give the space relevance.

The redevelopment has been designed to create a safe space by:

- » Creating attractive spaces across the site to entice people and ensure that areas remain well used
- » Maximising the opportunity for surveillance during day-to-day activities
- » Enhancing access control by providing lighting within the public domain to guide people to preferred safe routes
- » Providing a clear distinction between public and private space
- » Ensuring the area is well maintained, which sends a clear message that people care about and notice what happens in the area.

An awning is provided along the northern, eastern and southern façade of the building at ground level. This awning contributes to the visual aesthetics of the building and also provides protection from the elements for pedestrians moving along the footpath, or utilising the area of public open space.

In addition to a singular awning, individual awnings are also proposed above each individual shop will operate in such a way that allows them to be lowered at particular times when a security shutter is required for shopfronts. This combination of an awning and shutter feature provides a positive visual addition to the public domain which is an aesthetically improved alternative to the ad-hoc traditional roller shutters currently in place at the other centres.

The proposal positively contributes to the public domain and streetscape by fronting directly onto Dutton Lane with active uses and avoidance of blank walls at the ground level.

Windows and openings along the northern, eastern and southern facade create articulation and visual interest for passing pedestrians. Pedestrian fencing proposed along sections of the northern, eastern and southern façade will provide a path of travel through the public domain for pedestrians and will also provide visual and active separation between the pedestrian component of the public domain and the vehicle environment on Dutton Lane.

Internal amenity

The internal amenity of retail and commercial developments has been considered to ensure occupants have high quality internal spaces. Many of the proposed retail tenancies within the development face outwards towards Dutton Lane and have windows and/or doors leading out onto the footpath. This feature provides a high level of natural light throughout the daytime, which reduces the requirement for artificial lighting.

The public open space located at the eastern end of the development will receive sun throughout much of the morning period, which will ensure people sitting within the public domain have good solar access at points of the day. The area to the east although built up, is primarily comprised of lower height buildings which will not result in significant overshadowing of the open space area.

The internal car parking area on Level 1 and rooftop car parking on Level 2 is designed to provide undisturbed sightlines and maximum visibility, as well as safe vehicle manoeuvring.

5.2 Assessment of social and economic impacts

5.2.1 Employment and economic impact

An Economic Impact Assessment has been prepared by HillPDA to ascertain the economic impact for the retail component of the proposed development on the surrounding area and retail hierarchy. The Assessment also investigated the demand for retail uses within Cabramatta which the proposed development seeks to provide.

Overall, the proposal has a range of additional economic benefits. These include:

- » Construction is estimated at \$16m for the development, providing wider direct and indirect multiplier benefits of \$46m
- » The creation of 64 job years directly in construction and a further 199 job years indirectly during the construction process
- » Supporting an estimated 127 full time and part time jobs in retail and commercial services
- » Increase in shopper convenience by providing an accessible retail and commercial development is a direct response to growth in demand
- » Increasing shopper convenience by providing onsite parking for 275 cars
- » Increased price competition.

Demand

Cabramatta plays an important unique role providing specialty Southeast Asian food non-food products. There are around 560 business units in Cabramatta of which around 430 or 77% are retailers including personal services, supermarket, fabric stores, specialty food and non-food and cafes / restaurants. Hill PDA estimates that the total floorspace occupied by retailers in Cabramatta to be approximately 41,000 sqm.

Cabramatta Major Centre is a destination type centre providing a unique and important role in the provision of Asian specialty food and non-food products. The high concentration of persons within the primary trade area (50%) born in Asia reveals a high demand for such retail outlets. Market research has revealed that there is a high demand for retail floorspace within Cabramatta in highly visible areas. This is evident in the high sales rates and rents experienced within Cabramatta which in some cases is two to three times higher than strip shops in other centres within the Cities of Fairfield and Liverpool.

Overall, the study found:

- » Residents in the primary trade area will demand an additional 7,800sqm of retail floorspace by 2021. In accordance with the type of retail services currently present within Cabramatta 5,229sqm or 67% will be additional fast-food stores and restaurants, hotels and clubs, clothing, specialty food, supermarket / grocery store, personal services and household goods floorspace. The development of the retail floorspace within the proposed development will alleviate some of this growth in demand by providing up to approximately 3,000 sqm of retail floorspace.
- » Residents in the secondary trade area (STA) will demand an additional 141,350sqm of retail floorspace between 2011 and 2021. The 3,000sqm of retail floorspace proposed within the development will meet a small proportion of this growth in demand without any adverse impacts on surrounding centres and without constraining any further development in the STA.

Retail impact

The proposed development is expected to achieve a turnover (total retail sales inclusive of GST) of around \$18m based on an average trading level of \$6,000 per square metre.

The \$18m will be redirected from existing retailers in Cabramatta and from competing centres. Immediate or "point-in-time" impacts on existing retailers in Cabramatta and on existing centres will be below 5% loss in turnover.

These levels of impacts are considered insignificant.

Furthermore, due to population growth and growth in real retail spend, existing retailers in Cabramatta and all retail centres in the locality are expected to enjoy some growth in trade. Cabramatta's trading performance will increase by 9.4%. Centres in the locality will enjoy an average 4.4% growth in trade over the next three years notwithstanding the impacts from the proposal. No one centre will experience a loss in trade over the next 3 years.

These impacts are considered by HillPDA to be acceptable under Section 79C of the EPA Act in relation to economic impacts.

Consistency with Council's Centres policy

The proposed development complies with Council's Centres Policy because it will:

- » not result in unacceptable level of impact on the range of services provided in other existing sub-regional centres in Fairfield;
- » not result in a reduction of the range of services provided in nearby neighbourhood centres;
- » reduce some (albeit a minor amount) of escape spending from Fairfield (it will also increase expenditure coming into Cabramatta from other Local Government Areas); and
- » improve the viability of Cabramatta by responding to growth in demand; and will result in a positive net community benefit in the locality by replacing the at-grade open car park in the middle of Cabramatta shopping centre with active vibrant retail space.

5.2.2 Accessibility

The proposal ensures an accessible path of travel is available to all levels and public areas of the building and also ensures the provision of adequate accessible car parking facilities.

The site will be primarily accessible by pedestrian movement via Hughes Street, an external pedestrian linkage between Dutton Lane and Park Road, a secondary linkage between Park Road and Dutton Lane and through the Lucky Plaza Arcade from Hughes Street.

The site is also easily accessible from Cabramatta Train Station which is located approximately 200 metres away.

5.2.3 Crime Prevention Through Environmental Design

Based on consultation with the Cabramatta Local Area Command (LAC), this part of Cabramatta has previously been considered unsafe (at certain times of the day) due in part to the lack of public activity. There are also limited opportunities for passive surveillance at this location and a dominance of vehicle.

The proposed development, at this height and scale, improves the public domain through new opportunities for passive surveillance.

Consultation with the LAC revealed that crime in Cabramatta has been a problem historically, and although significantly improved through increased security provision and urban design, is still a major problem today.

Design of the proposed development has considered CPTED principles to ensure the design of the development enables a safe environment for those working at, or visiting, the site.

The key principles of CPTED, and how the design of the development responds, are discussed below.

Surveillance

This principle helps to create environments where there is plenty of opportunity for people engaged in their normal behaviour to observe the space around them. Surveillance deters people from partaking in anti-social or potentially illegal behaviour through a belief that they may be witnessed.

With this principle in mind, the following issues have been addressed:

- » The proposed development provides retail facilities on the ground floor that face outwards towards Dutton Lane. These facilities will have large glass installations (in the form of windows and/or doors) to ensure clear sight lines between public and private space are provided. This will enable a high level of natural surveillance between private property and the public domain (and vice versa).
- » At the detailed design stage, lighting design will ensure Dutton Lane is well lit so that clear sight lines across the site are clear. Lighting will be provided so to ensure that entrances, exits, service areas, pathways and other public areas are well lit after dark when they are likely to be utilised for access to night time premises i.e. restaurants and cafes. Lighting will be installed so to ensure a safe level of illumination at the ground level and public domain around the buildings with an emphasis given to preferred routes to encourage their usage by pedestrians, and supplementary lighting at building entry points. Lighting provided to complement the proposed development will spill across public space to ensure lighting provides linkages to other parts of Cabramatta Town Centre.
- » Hard and soft landscaping will be implemented within the public domain around the site to ensure an enjoyable experience for those visiting or working in the development. The design and location of such landscaping will be approached in such a way that will ensure concealed areas and blind spots (hiding spaces) are not created.
- » Car parking facilities on the first and second floor of the development will be very well lit (both naturally and artificially) and painted in a light tone of colour to ensure that active surveillance is optimised at all times of the day and night.
- » Integrated Closed-Circuit Television (CCTV) infrastructure is currently installed across the Cabramatta Town Centre to deter individuals and groups from partaking in illegal and/or anti-social behaviour. It is proposed that the development have High Definition (HD) CCTV infrastructure installed which is capable of undertaking a tilt, pan and zoom function. CCTV will be installed around high risk areas (toilets and building entries and exits) and around entrances and exits to the building. This infrastructure will ensure areas of the public and private domain (where appropriate) will be under surveillance.

Access control

This principle ensures there are legible boundaries between public and private space and also enables people to be channelled through a particular area towards the most active and well observed space. Provision of natural access control limits access and increases natural surveillance to restrict criminal intrusion, especially into areas where they will not be easily observed. This

principle endeavours to increase the effort required to commit a crime and therefore, reduce the potential for it to happen.

With this principle in mind, the following issues have been addressed:

- » Lighting within the public domain will be installed so as to guide people along a particular route.
- » Partitions of fencing will be installed along the paths surrounding the proposed development, to direct people towards designated pedestrian crossings and reduce instances of jay walking. This will enhance pedestrian safety and enhance traffic flow through the site.
- » Ensuring spaces across the site are attractive through the use of hard and soft landscaping and lighting etc will entice people travelling through the site to follow a particular route. This will ensure that areas remain well used by more than one person at once.
- » Access to areas of high risk (namely bathroom facilities and a gap between the western boundary of the proposed development and the neighbouring multi-storey car park) will be gate controlled to ensure that access can be controlled at times of high risk, likely night time.
- » The shops facing west onto Dutton Lane are to be accessed directly from the street frontage rather than through the inside of the arcade. As a result, these stores will be able to operate after the internal stores have ceased operation for the day. This enables access to particular stores to be restricted, while allowing others to trade for longer hours (likely reserved for food and beverage developments). This arrangement will also ensure a high level of surveillance into the evening.

Territorial reinforcement

This principle aims to ensure a clear distinction between public and private space is provided. This ensures legitimate occupants have a sense of ownership and will notice, and likely challenge people who do not belong within a particular space.

With this principle in mind, design features will be incorporated to ensure there is a clear distinction between public and private space.

Where areas are restricted to solely private access, gates and/or fences and signage will be installed to effectively communicate the boundaries. Additionally, outside of the main frontage of the development where public space is evident, paving distinctions, awnings and street furniture will indicate the areas intended usage for the public.

Space management

This principle aims to ensure an area is well maintained, which sends a clear message that people care about and notice what happens in the area. To ensure the development and surround spaces are well managed and maintained, regular maintenance of landscaping will be undertaken. Additionally, any damage or graffiti will be repaired / removed promptly, areas will be kept clean and any routine maintenance (such as the replacement of light fittings) will be undertaken in a timely manner to ensure the development does not ever appear in a state of poor condition.

5.3 Assessment of site suitability

The subject site is suitable for the proposal development. The development replaces an ageing at-grade car park with new retail and commercial space, as well as above ground parking facilities. It provides a contemporary and complimentary addition to the Cabramatta Town Centre in a highly accessible location.

The proposed development replaces a singular use, with a development that offers parking facilities, a diversity of leasable area suited to a variety of different purposes, and open space within the public domain to compliment the operation of the new development.

5.4 Assessment of public benefits

5.4.1 Public benefit

The proposed development provides significant public benefit through more efficient and effective use of the land with minimal environmental impacts.

The proposal replaces an older at grade car park facility with an attractive new commercial and retail building with car parking facilities. The increased diversity of uses on the site will provide a new attraction for residents of Cabramatta as well as visitors from out of the local area. The development of new retail facilities will facilitate the continued development of Cabramatta as a cultural hub in South West Sydney. Further, new toilets will be incorporated in the retail building replacing the existing toilet blocks, a new lift to provide easier access from the new and existing car parks and a public open space area.

The proposal also creates significant improvements to the pedestrian and vehicular environment within Dutton Lane. The proposal provides a safer pedestrian environment, while also ensuring the traffic occurrence. The majority of car entering the site will utilize the ramp at the north-eastern corner of the development to access car parking facilities in the upper half of the building. This action will reduce the proportion of vehicles driving around Dutton Lane seeking parking facilities. The implementation of more uses across the site increases the opportunity for passive surveillance.

5.4.2 Community information and feedback session

A community information and feedback session was held by Fairfield City Council on Thursday 5th September 2013 at the Cabravale Leisure Centre, Broomfield Street, Cabramatta.

The session was between the 5:30pm to 8:30pm.

Public notification was provided through advertisements placed in the local and ethnic newspapers as follows (Refer to Appendix J):

- » Advance and Champion Newspapers on 28th August 2013 and 4th September 2013;
- » Dan Viet Vietnamese newspapers on 30th August 2013; and
- » Australian Chinese Daily on 30th August 2013 and 4th September 2013.

In addition, the Cabramatta Chamber of Commerce and Cabramatta Business Association were also notified on 22nd August 2013 to inform their members of Community Information and Feedback Session.

Attendees were invited to fill in feedback forms and/or provide verbal feedback to members of Fairfield City Council and the project team in attendance. A representative of the planning and traffic consultants from the project team was available to answer questions from members of the

community. A series of boards were displayed around the room with information about the project (Refer to Appendix K).

A summary of issues raised at the Community Information and Feedback Session, and how the proposal responds, is provided in the Table 8 below. 9 feedback forms were filled in at the session.

Overall, it is considered that issues raised by members of the community can be addressed during the future operation of the facility.

Table 8 Community Feedback and Responses

Issue	Comment	Response
Commercial / retail uses	» Consider building retail / commercial components on top of car park with car parking on ground floor (so as to not create draw business away from surrounding shops)	» The car parking has been located on Levels 1 and 2 in order to activate the pedestrian realm and public space surrounding the site. » There is currently significant conflict between vehicles and pedestrians using Dutton Lane, therefore the proposal seeks to divert cars away from navigating through the entire length of Dutton Lane by providing an express ramp to Level 2 and links to the adjacent car park. In this regard, the proposal creates an opportunity for retail / commercial components on ground floor, to complement surrounding shops and create activity on the ground level. » Shops on the ground floor assist in achieving CPTED principles such as surveillance and activation.
	» Too many small shops and not enough medium-large shops.	» The proposed use of the ground floor as retail proposed approximately 30 smaller (specialty) shops which front the inner arcades and the outer facades of the proposal. These smaller uses are considered appropriate to complement the existing uses and provide space for shops where there is demand. » The ground floor design has flexibility to cater for small to medium/large shops. The ultimate configuration will be dictated by market demand.
	» Good to see new, clean shops that will result in more jobs for local residents.	» Noted.
	» Retail shops on ground floor will impact businesses surrounding the	» The proposal has been designed to activate the periphery of the

Issue	Comment	Response
	<p>site because shoppers will stay in the centre rather than access existing shops surrounding the site.</p>	<p>development, but locating shop front with access only from the outside / Dutton Lane, thereby drawing people to the outdoors.</p> <p>» The proposal includes a new public open space which is considered to act as an attractor to visitors and shoppers. In this regard the proposal is drawing shoppers into the public realm.</p> <p>» It is noted that an Economic Study has been carried out by HillPDA which has found there will not be detrimental impacts on surrounding shop owners. The HillPDA report found that demand for additional retail floor space between 2011 and 2021 will be in the order of 141,350sqm new floor space required to meet demand from the secondary trade area (and 7,800sqm of new floor space for the primary trade area). The proposed introduction of some 3,000sqm of retail floor space as part of the proposed development will contribute only a small proportion of this required floor space to support the growth in demand, without any adverse impacts on surrounding centres.</p> <p>» The HillPDA report found that immediate or "point in time impacts on existing retailers in Cabramatta will be below 5 per cent loss in turnover, which is considered insignificant. However, despite this, due to population growth and growth in real retail spend, existing retailers in Cabramatta are expected to enjoy growth in trading performance in the order of 9.4 per cent. Given the anticipated growth, and the low level of impact expected, the HillPDA assessment finds that <i>"these impacts are acceptable under Section 79C of the EP&A Act in relation to economic impacts."</i></p>
Design	<p>» Like the look of the building and upgrade of what is currently an</p>	<p>» Noted.</p>

Issue	Comment	Response
	"eyesore" in Cabramatta Town Centre	
Dutton Lane and Hughes Street junction	» Intersection of Hughes Street and Dutton lane is a problem.	» The proposal seeks to improve the flow of traffic and traffic performance of Dutton Lane and the intersection at Hughes Street, by modifying the intersection arrangement (as described in Section 3.1.5 of this SEE). The proposed changes will significantly improve the functionality of the intersection.
Pedestrian movements	» Consider pedestrian flow and safety at crossings when crossing Dutton Lane.	<p>» Safety and pedestrian priority has been a key consideration in the design. The introduction of new proposed raised/marked pedestrian crossings, as well as fencing around the perimeter of the proposed development, will direct pedestrians to safe crossing points and divert pedestrians from crossing at unsafe sections.</p> <p>» The internal arcades in the new building, together with fencing around the perimeter, has been designed to maintain effective pedestrian flows and connectivity between the surrounding arcade malls.</p> <p>» Broadly, the improved traffic flow (with cars accessing the rooftop via an express ramp and multiple exit options out of the car park) means that fewer cars will need to navigate the entire length of Dutton Lane to exit, thereby diverting some vehicle movements and making the area more pedestrian-friendly.</p>
	» Dutton Lane is very busy; will the proposal create chaos?	» As noted above, the proposal is designed to improve traffic flows, divert pedestrians to safe crossing points and reduce the number of vehicles and trucks using Dutton Lane.

Issue	Comment	Response
Car parking	<ul style="list-style-type: none"> » More car parking required; Is it possible to increase the provision of car parking further? » More parking will benefit business in Cabramatta. 	<ul style="list-style-type: none"> » The proposal (as presented at the Community Information Session) included 240 car spaces. Based on feedback for the strong demand for car parking in this area, design changes were made to increase the car parking provision to 275 spaces.
	<ul style="list-style-type: none"> » Question whether Council are removing existing car spaces that service surrounding shops. » Question whether enough car spaces have been provided to service the proposed new shops. 	<ul style="list-style-type: none"> » The proposal has been assessed against the Cabramatta DCP guidelines. » All existing 157 car spaces will be replaced, as well as 9 spaces removed from Hughes Street. » The proposal provides an additional 109 spaces to service the proposed uses. Under the Cabramatta DCP, a total of 133 spaces would be required. However, this rate can be discounted by 40% if Section 94 contributions are paid for some off-site provision according to the DCP. » As illustrated in the DCP compliance table, the proposal has been designed to accommodate all required parking spaces in accordance with the controls of the DCP. It is noted that the DCP controls provide that only 30% of required car parking should be provided on-site. In light of changes to the Section 94 plan and the feedback from the community (that more car parking in Dutton Lane is required), the proposal provides all required parking on-site and proposed associated improvements to assist in the flow of traffic and intersection arrangements. This is considered to be an appropriate practical solution.

6 Conclusion

The proposed development provides an effective use of the subject site. It replaces a ground level car park that is currently underutilised, given its location within Cabramatta Town Centre.

The proposal will be a high quality retail/commercial development with all existing car parking facilities, public toilets and loading bays replaced. The proposal also responds to the need generated by the new development.

The proposal will result in both improved and increased pedestrian activity which will stimulate and complement the existing operation of businesses within the Cabramatta Town Centre.

The proposal has planning merit as it:

- » responds to the site characteristics with a design appropriate to its context
- » is compliant with key planning controls
- » demonstrates public benefit

Given the location of the subject site, in close proximity to Cabramatta railway station, the proposal contributes to the provision of retail and commercial space and employment opportunities in close proximity to public transport services and a large town centre.

This SEE has demonstrated that the proposal complies with the relevant provisions of Fairfield LEP 2013 and has no significant adverse environmental effect, particularly in relation to height, building envelope, landscaping, and stormwater and surrounding amenity.

In light of the merits of the development, the proposal is considered worthy of support and recommended for approval.

Appendices

A DCP compliance assessment

Cabramatta Town Centre Development Control Plan No. 5/2000

Section	Control and performance criteria	Assessment
General controls		
Application of this DCP chapter	This plan applies to all land shown in Map 1 as the "Cabramatta Town Centre" and which is zoned: Mixed Use B4	Noted – This DCP applies to the site.
Precinct	Precinct 2: Commercial and Larger Retail, Dutton Lane Car park	Noted – The site is located within Precinct 2.
Precinct 2 objectives	Encourage medium scale and car based retail activities such as Variety Stores and Supermarkets up to 2,500sqm floor space of a scale compatible with the central area of the Town Centre.	Complies – The proposed development includes medium scale and car based retail activities which complement the existing operation of the Cabramatta Town Centre and provide a mix of spaces for variety stores as well as other larger scale retail operations.
	Encourage the development of commercial offices of between three and five storeys in specific locations.	Generally complies – The proposed development provides office spaces on the second floor of the development.
	Ensure that the siting and bulk of buildings contributes to the existing or proposed character of the precinct.	Complies – The proposed building's bulk corresponds well with the proposed character outlined within the character statement. A mix of retail facilities are proposed on the ground floor which have considered the desired character.
	Encourage the development of new public open space to facilitate celebration of cultural diversity and temporary markets.	Generally complies – The proposed development does not offer landscaped open space for the purposes of markets, however the proposal offers an 'open forum' retail area with strong links to surrounding retail facilities and nearby outdoor public open space in the form of an open air plaza.
	Ensure adequate safe, convenient and accessible car parking and provide adequate on-site loading and unloading facilities.	Complies – Safe, convenient and accessible car parking for retail operators and members of the public is provided in car parking on the first and second floor of the proposed development. On-site loading and unloading facilities are also provided to compliment retail facilities. CCTV is to be provided throughout the car park and at safety "hot spots" like near entries/exits.
Precinct 2 Desired	Precinct 2 Desired Character statement	Complies – » The proposal involves the development of

Section	Control and performance criteria	Assessment
Character statement		<p>small and medium sized retail developments and does not comprise of any 'big box developments'.</p> <ul style="list-style-type: none"> » The proposal provides a development with strong pedestrian linkages to surrounding developments within the Cabramatta Town Centre and public spaces. » The development provides a strong pedestrian linkage with Freedom Plaza, John Street and Park Road. » The pedestrian linkage provided between the eastern edge of the proposed development and Park Road incorporates a component of new public space with tables and chairs, benches and plantings. Within this area a Fairfield City Council History Wall will be provided which will reinforce the history and culture of the local area. » Public space is proposed at the eastern end of the development which provides tables and chairs, benches and soft landscaping to ensure an attractive, vibrant and safe meeting place for pedestrians. Pedestrian fencing along proportions of the northern, eastern and southern perimeter of the building provide definition between the footpath and Dutton Lane and also make the pedestrian area around the proposed development much safer. » Pedestrian movement will be allowed through the centre of the development in a northerly, southerly, easterly and westerly direction. Pedestrians consequentially will have a high level of access to the public and private domain across the site.
Precinct 2 Controls		
FSR	2.5:1	Complies – Refer to Section 4.1.2 of this SEE.
	<p>Performance criteria:</p> <ul style="list-style-type: none"> » Building bulk and scale to be appropriately related to site area and frontage and desired character for this precinct. » An open forum retail development is recommended. 	<p>The proposed development has been designed to ensure that the scale and bulk is respectful of existing surrounding developments. An effort has been made to design the development so to screen parts of the multi storey car park to the west, while still ensuring the proposed building does not overshadow existing developments or public space. The incorporation of coloured panels to the facades of the building screens the new parking facilities in the upper level of the development which further reduces the appearance of bulk and scale. It is considered that the design</p>

Section	Control and performance criteria	Assessment
		The proposal incorporates an arcade style retail development that has strong links to outdoor space surrounding the development through the provision of key access ways on the northern, eastern and southern perimeter. These features ensure a strong link between the inside of the retail development and the outdoors.
Height	<p>3 storeys / 10 metres</p> <p>» Buildings on the development site located between Dutton Lane and Council's multi-deck car park may be erected to a height of up to 5 storeys or 16 metres above existing ground level provided that at least 50% of the site is developed as landscaped public open space.</p>	<p>Non-compliance justified – the proposal is generally within 10 metres with the exception of the fire-isolated stairwells (4) which exceed the height limit by 0.85 metres and the lift lobby canopy which exceeds 10 metres by 0.10 metre. A clause 4.6 variation has been sought (refer to Section 4.1.2 of this SEE).</p>
	<p>Performance criteria:</p> <p>» The height of a building should relate to the public street, be of a compatible scale with surrounding development and not cause additional overshadowing of pedestrian/public areas between 9am and 3pm in winter months.</p> <p>» Buildings to the east of the multi-deck car park should be erected to a height of 4 to 5 storeys to assist in screening the visual impacts of the existing structure.</p>	<p>Complies – The proposed development is of similar heights to existing retail developments surrounding the site, to ensure minimal impact on the solar access available in the public domain.</p> <p>The proposed building is located to the east of the multi-deck car park however it is proposed at three storeys rather than 4 – 5. This will provide an element of screening of the existing car park structure.</p>
Vehicle access denied	DCP map indicates where vehicle access is denied.	<p>Complies – The primary point of access to the development is via an existing roadway accessed off Hughes Street.</p>
Car parking – rates	<p>Office and Business use:</p> <p>» 1 space per 25sqm of gross leasable ground floor area</p> <p>» 1 space per 40sqm of gross leasable floor space above ground level.</p> <p>Retail, Restaurant, Cafes,</p>	<p>Partially complies (justified) – Based on the proposed development, the following car parking requirements have been calculated:</p> <p>1 / 25sqm x 2,995 sqm (retail) = 119.8</p> <p>1 / 40sqm x 505 sqm (office) = 12.6</p> <p>Therefore, total parking spaces required to service the proposed new uses is 133 spaces (rounded).</p>

Section	Control and performance criteria	Assessment
	<p>Refreshment Rooms:</p> <p>» 1 space per 25sqm of gross leasable floor area.</p> <p>Where contributions are paid for a centralised car park (in lieu of on-site parking) the above parking requirements may be reduced 40%.</p>	<p>The existing at grade car park provides 157 spaces.</p> <p>The proposal seeks to remove 9 spaces (existing on-street) from Hughes Street.</p> <p>Therefore the total number of car parking spaces required is calculated as:</p> <div><div>157</div><div>9</div><div>133</div></div> <p>Total req. = 299</p> <p>Total provided = 275</p> <p>The proposal provides a shortfall of 24 spaces on-site. It is proposed to pay a s.94 contribution to account for the shortfall in spaces.</p> <p>The CDCP provides that where contributions are paid for centralised car parking(in lieu of on-site parking), the required parking rates may be reduced by 40%.</p> <p>Therefore, the required amount to be paid in contributions (24) is reduced by 40% to give a total shortfall of 14 spaces (after the 40% discount).</p> <p>It is therefore proposed that a section 94 contribution is paid for 14 spaces not provided on-site.</p>
Car parking – controls	<p>Access to parking areas and servicing areas shall utilise rear lanes and parking areas shall not be located at the street frontage.</p> <p>Vehicular and loading facilities shall not be located on the street frontage. Where it is necessary, this access should be well integrated into the streetscape.</p> <p>Development shall provide access for disabled persons, as per the</p>	<p>Not applicable.</p> <p>Complies – The proposal integrates existing service vehicle areas within Dutton Lane into the proposal through the provision of dedicate loading bay areas distributed around Dutton Lane.</p> <p>Some existing loading bays currently located at the eastern end of the site have been relocated to Hughes Street. Considering the eastern end of the site is being proposed as a public domain area for community/public use, the provision of loading bays in this part of the site was considered unsightly and dangerous. The eastern end of the site links to Freedom Plaza and is pedestrian-focused. The re-distribution of loading bays, together with other proposed improvements to pedestrian crossing points, will create a safer and more pedestrian friendly environment.</p> <p>Complies – A lift is provided to access upper levels of the proposal. Ramps accommodate level</p>

Section	Control and performance criteria	Assessment
	relevant Australian Standards.	changes between the footpaths, crossing points and internal spaces.
Car parking – criteria	<p>Performance criteria:</p> <ul style="list-style-type: none"> » Access for public car parking shall be provided at the required rate. Any charge for public access will not be permitted. » Garage doors shutters and grilles should blend with the appearance of the building. » Parking above ground level should relate to the scale, massing, proportions, materials and finishes and should be designed as an integral part of the main facade. » Not more than 30% of required car parking shall be permitted on site. A Section 94 Developer Contribution is to be paid for the parking not permitted on site. 	<p>Complies – Parking facilities have been provided at the required rates.</p> <p>Complies – Shutters are proposed along the northern, eastern and southern façade of the building, which will be designed to ensure they can be lowered down and lifted up to enable usage as an awning when a shutter feature is not required. This innovative feature provides a visually attractive alternative to traditional roller shutters and will enhance the visual appearance of the building.</p> <p>Proposed parking facilities above ground level have been designed to be integrated within the development footprint; external treatment and finishes (perforated aluminium) will improve appearance of the development so that it responds to the desired character statement for the precinct. Large red panel installations will be installed on the facade of the first and second levels of the proposed development to provide an aesthetically pleasing feature and a screen for parking facilities from the outside.</p> <p>Non-compliance (justified) – The development provides 275 car parking spaces, which is a shortfall of 24 spaces (or <u>14 spaces</u> after the 40% discount is applied – refer above). The shortfall is proposed to be accounted for by a Section 94 payment given the proposal is a centralised car park.</p> <p>The requirement to provide a maximum 30% of required car parking spaces on-site (and pay the remainder in contributions) is considered unreasonable in the circumstances of the site, for the following reasons:</p> <ul style="list-style-type: none"> » Council’s Section 94 Plan does not provide sufficient alternative car parking in the event that 70% of the required parking is not provided on-site. » The objective of providing a centralised car park to service Cabramatta Town Centre is still achieved by the proposal.

Section	Control and performance criteria	Assessment
		<p>» Despite on-site provision of car parking, the design of the proposal results in improved traffic arrangements and traffic flow through Dutton Lane.</p> <p>» Despite on-site provision of car parking, the design of the proposal still achieves related objectives such as activation of the pedestrian and public realm.</p> <p>» The provision of only 30% spaces on-site would be an inefficient use of space.</p> <p>» Community feedback indicated the need for more car parking in Dutton Lane.</p> <p>Each of these issues is discussed in greater detail below, where reasoning for non-compliance with this control is justified.</p> <p>1. Section 94 Plan – able to meet the intent and objectives of the S. 94 Plan</p> <p>Council's Section 94 Plan for car parking in Cabramatta dates back to 1999. The Section 94 Plan states that in Cabramatta;</p> <p><i>"the developer may choose to pay a contribution for each space which he or she does not wish to provide on site.</i></p> <p><i>In most cases the payment of car parking contributions is the choice of the developer.</i></p> <p><i>The developer may choose not to pay a car parking contribution simply by providing all required car parking on site.</i></p> <p><i>However, there are a few exceptions to this in Cabramatta, where the Cabramatta Town Centre DCP has identified some precincts where on-site parking is not permitted for traffic, safety and/or amenity reasons.</i></p> <p><i>Car parking contributions collected in the Cabramatta Town Centre will be spent in the contributions catchment area.</i></p> <p><i>In some precincts within the Cabramatta Town Centre, on-site car parking is either not permitted or limited, hence payment of a car parking contributions is necessary. In other precincts payment of a contribution is optional.</i></p> <p><i>Car parking contributions are to be applied to the construction of a multi-deck car park off Cabramatta Road in the block bounded by John Street, Hill Street and Cabramatta Road."</i></p> <p>Regarding the new multi-deck car park that was to be provided through contributions payable, the</p>

Section	Control and performance criteria	Assessment
		<p>car park was to provide 424 car parking spaces at an estimates cost of \$8 million plus land acquisition of at least \$2.4 million.</p> <p>In addition;</p> <p><i>"traffic signals and some road access improvements remain to be completed at the Dutton Lane car park. These works and necessary land acquisition are estimated to cost \$510,000."</i></p> <p>Land for the multi-deck car park referred to in this plan (being in the block bounded by John Street, Hill Street and Cabramatta Road) has been acquired. However, Council no longer plans to construct a multi-deck car park in this location due to surrounding development constraints. This location has been constructed and is operating as an at-grade car park.</p> <p>In the absence of an alternative Section 94 Plan or alternative supply of car parking spaces in Cabramatta, the proposal is to provide 100% of car parking spaces in the Dutton Lane car park (i.e. 275 spaces plus contributions for the 24 shortfall) is considered necessary to accommodate traffic/shopper convenience demand.</p> <p>Furthermore, the objectives/intent of the Section 94 Plan – to provide a centralised car park to service Cabramatta Town Centre – is still being achieved regardless of the non-compliance with the DCP. It is also considered that the proposed development helps Council to achieve its objective and at the same time, does not prevent Council from achieving any of its long term objective of its car parking strategy in its Section 94 Plan. While Council considers possible options for alternative sites for a multi-deck car park (to potentially replace the previous plan to develop a car park in the block bounded by John Street, Hill Street and Cabramatta Road), this proposal is providing an immediate solution to necessary car parking in a convenient location and has the additional benefits of:</p> <ul style="list-style-type: none"> » improvements to traffic flow in and out of Dutton Lane » improvements to the amenity of the area by providing a well-designed and attractive building » providing significant public open space, linked to existing pedestrian-only areas in Cabramatta, to improve the public realm » achieving objectives such as safety and activation.

Section	Control and performance criteria	Assessment
		<p>2. Traffic arrangements will be improved notwithstanding the provision of over 30% of car parking on-site</p> <p>The Traffic Impact Assessment has found that traffic arrangements entering Dutton Lane (off Hughes Street) are already operating at poor levels of service. Although the proposal will add spaces to the precinct, the series of traffic improvements proposed as part of this DA will, in combination, create improvements to traffic flow and safety.</p> <p>Specifically;</p> <ul style="list-style-type: none"> » Intersection modifications at the Highest Street / Dutton Lane junction will ease queue times and improve right hand turn movements out of Dutton Lane into Hughes Street (east) » Provision of an access ramp directly to Level 2 of the proposed car park building, with boom gates at the top of the ramp, provides additional waiting space for vehicles, reducing the number of cars waiting in Dutton Lane and out into Hughes Street » Provision of electronic signals to inform motorists of available car parking spaces will help to improve the flow of traffic internal to the car park and direct vehicles to available spaces more quickly » Access ramps connecting the proposed and existing multi-deck car parks will enable vehicles to find a space without the need re-enter Dutton Lane, thereby removing cars from Dutton Lane and enhancing the pedestrian environment » By connecting the car parks, motorists have a choice to exit the Dutton Lane car park west out of the existing multi-deck car park and into Hill Street, thereby reducing number of vehicles exiting the proposed car park and then being required to navigate around Dutton Lane to exit. <p>Given the above improvements, it is considered that the Dutton Lane car park is capable of accommodating the stated car parking spaces without causing additional traffic issues. The Traffic Impact Assessment undertaken to assess the traffic impact of the proposed DA, found that the proposal will not result in detrimental traffic movements and should be supported on traffic grounds.</p>

Section	Control and performance criteria	Assessment
		<p>3. Additional public benefits</p> <p>Finally in relation to Section 94 contributions, it should be noted that the proposed development incorporates significant public benefits and improvements. In addition to improved pedestrian raised crossing points over Dutton Lane and the intersection modifications identified above, the most significant public benefit associated with the proposal is the provision of approximately 500sqm of high-quality, useable outdoor space as public domain.</p> <p>As shown in the Architectural Plans, the proposed seeks to construct a new building within the Dutton Lane precinct. The footprint of the building allows for the continued operation of Dutton Lane, provides loading bays, and accommodates a pathway around the perimeter of the building. However, rather than extend the building footprint to the Dutton Lane carriageway, the proposal seeks to create an outdoor public space in the order of 500 sqm to be dedicated to public use.</p> <p>By locating public space at the eastern section of the site, adjacent to existing pedestrian-only thoroughfares, the proposal will revitalise the Dutton Lane precinct which has been dominated by vehicles. The public open space will incorporate outdoor furniture, some landscaping and treatments (including paving, awnings and the like) that will make the space attractive to visitors. Furthermore, retail uses at the ground level have the additional potential to activate the space and provide amenities that complement surrounding uses but, importantly, provide an incentive for people to stay and use the space. This has multiple flow on public benefits related to safety, surveillance and a sense of space and community.</p> <p>It is noted that the provisions of the <i>Indirect (Section 94A) Development Contributions Plan 2011</i> covering Fairfield LGA enable works-in-kind or alternative methods of payment of S.94 Contributions, such as that which is proposed by locating car parking spaces on-site and making contributions toward any on-site spaces that cannot be accommodate (i.e. 24 spaces).</p> <p>4. Efficiency in land use</p> <p>It is not practical to build a structure to replace only 166 spaces which covers 1.5 floors. It is considered a better and more efficient use of land to complete two floors to enable the built form to be aesthetically pleasing, as well as create car</p>

Section	Control and performance criteria	Assessment
		<p>parking spaces that are in demand in the area.</p> <p>Summary</p> <p>In summary, the proposed car parking provision arrangements – being to provide the majority of required car parking on site (and paying a section 94 contribution for the shortfall of 14 spaces) – is considered reasonable.</p> <p>While this proposed car parking provision would be inconsistent with the 30% maximum DCP control, it is justifiable based on:</p> <ul style="list-style-type: none"> » Limited viable alternative car parking facilities in the vicinity of the site and an impractical option identified in the current planning controls » Suitability of the site (given proposed traffic movement improvements) » Overarching benefits of the proposal (in terms of design, amenity and pedestrian activity) for Dutton Lane and the Dutton Lane precinct » Other proposed public benefits such as dedication of land for public open space » It is more efficient and practical to build a 2 storey structure, as opposed to a structure which covers only 1.5 floors, as completing 2 floors will enable the built form to be aesthetically pleasing and enable the provision of much needed car parking » Provides an immediate solution to the demand for car parking spaces in Cabramatta whilst still enable Council to achieve its long term car parking strategy under its Section 94 Plan. » The DCP is a guideline only and flexibility in the application of DCP controls can be considered on merit.
Multi-storey car parks	<p>Relevant controls in Chapter 12 for multi-storey car parks include:</p> <ul style="list-style-type: none"> » Multi storey car parks should be constructed at a scale and bulk compatible with surrounding buildings. » The proportions of openings and materials used in parking stations should reflect those of nearby structures. This may require a façade to be incorporated in the design of 	<p>Complies – The proposed development integrates within surrounding developments in regard to scale and bulk, and is compatible to the 2-3 storey scale of surrounding buildings. It is noted that the proposal is lower in height than the adjacent multi-deck car park.</p> <p>Complies – The proposed development provides parking facilities on the first and second level of the building. These facilities are partially screened through the provision of creative coloured aluminium panelling which enhances the visual appearance of the building's façade. The</p>

Section	Control and performance criteria	Assessment
	<p>some parking stations since many are designed with floor slabs exposed to the street.</p> <ul style="list-style-type: none"> » Multi storey car parking facades also provide an opportunity for the incorporation of public art elements. Where public art is proposed design concepts shall be developed and discussed with council through the development application process. » Where the zoning permits, owners of multi storey car parks should consider activation of the ground floor level by incorporating activities such as shops or offices. 	<p>proposed development significantly improves the appearance of car parking above ground level that characterises other multi storey car parking facilities in the area.</p> <p>Partially complies – Opportunities for public art have not been specifically provided along the façade of the proposed development. Notwithstanding, the incorporation of coloured panelling on the external façade of the development avoids the presences of large blank walls, and provides a feature in a colour of cultural significance.</p> <p>Complies – Shops and retail uses are provided at ground level below the mutli deck car park, allowing for activation of the public domain and the streetscape.</p>
Loading / unloading facilities	<p>Minimum number of loading bays required:</p> <ul style="list-style-type: none"> » Butcher, Fruit Store, Furniture Store/ Showroom, Pub/Licensed Club, Supermarket (400 sqm) – 1 bay (3.5m x 11m) » Office: 1 bay per 4,000 sqm for first 20,000 sqm (50% of spaces must be adequate for trucks) <p>Loading bays may be reduced in width to 3m and in length to 6m where the premises are to be serviced only by small vans and the like. This concession does not apply to premises with a floor space exceeding 200 sqm.</p>	<p>Complies – As described in Section 3.1.5 of this SEE, given the amount of retail space proposed the development generates the need for 1 additional loading space and is required to accommodate/replace all existing loading bays. The proposal includes two loading bays within the site area (i.e. on the development site side of Dutton Lane). The bays have capacity to accommodate 3 vans each (6 total). In conjunction with the proposed surrounding loading bay arrangements, the nett result in an increase by 1 van loading bay as required. Refer to Section 3.1.5 of this SEE.</p> <p>Given the types of uses proposed (small retail tenancies) a loading space for a van (measuring at least 6m) is considered appropriate.</p>
Setbacks	<p>There are no numerical controls for setbacks.</p> <p>Zero lot boundary applies where the existing buildings are immediately on the boundary of the street frontage.</p> <p>A variable setback applies where there is a variable building setback from the street boundary.</p> <p>Performance criteria:</p>	<p>Complies – Performance-based controls relating to setbacks in existing streetscapes.</p> <p>Surrounding development in the block includes the existing multi-deck car park built to lot boundaries. The proposal is set back at ground level to allow for awnings above public pathways and a forecourt area to the east.</p> <p>The building is setback by approximately 19 metres from the Dutton Lane carriageway (at the eastern end) and provide footpaths of between 1.4m to 4.0m on the northern and southern</p>

Section	Control and performance criteria	Assessment
	<ul style="list-style-type: none"> » Existing streetscape should be maintained, or enhanced where the existing streetscape is characterised by development on the street boundary. » Where there is a variable building setback from the street boundary, the existing streetscape should be maintained, or enhanced. 	<p>facades.</p> <p>It is considered that the proposed development complies with desired character for the precinct, given the provision of new, high-quality public open space.</p>
Pedestrian movement	DCP map indicates where pedestrian movement is encouraged.	<p>Complies – The proposed development provides strong pedestrian links (as per the recommended pedestrian movement in the precinct map) through the central spine of the site and provides linkages to surrounding retail facilities, existing public open space and to multiple street frontages on Dutton Lane.</p>
Building design	<p>Building design shall comply with precinct objectives, standards and desired character with respect to scale, bulk, setback, height and streetscape.</p> <p>Council may require a pedestrian connection through a development site to facilitate pedestrian access to the Dutton Lane car park.</p> <p>Lighting shall provide external illumination levels of at least 75 lux when measured 900mm above ground level at any point on the footpath or pedestrian movement routes and 40 lux in loading areas or car parking areas.</p> <p>Security measures in development creating right of ways, concealed loading/unloading, parking or service areas without open and direct view of at least 90% of floor area from the public street shall be provided at the applicant's expense including maintenance costs with closed circuit television surveillance (CCTV) connected to Council's</p>	<p>Complies – The proposed building's bulk, height and scale responds to the objectives outlined within DCP for precinct 2. The proposal incorporates a mix of retail facilities on the ground floor with a small proportion of office/commercial space on the first floor. The proposal incorporates 2 levels of parking spaces spread across level 1 and level 2.</p> <p>Complies – An undercover arcade pedestrian connection is proposed through the development, providing links to existing public open space and surrounding developments (including retail and car parking facilities).</p> <p>Will comply – Details regarding specific types and locations of lighting will be determined at the detailed design / construction certificate stage.</p> <p>Complies – The proposed development provides retail facilities on the ground floor that face outwards towards Dutton Lane. These facilities will have large glass installations (in the form of windows and/or doors) to ensure clear sight lines between public and private space are provided. This will enable a high level of natural surveillance between private property and the public domain (and vice versa). Where direct views cannot be provided, the development will have High Definition (HD) CCTV infrastructure installed</p>

Section	Control and performance criteria	Assessment
	<p>resistant, traversable by wheelchairs and prams and indicate changes of grade.</p> <ul style="list-style-type: none"> » Accessible paths shall be provided from accessible parking spaces and passenger loading zones or public streets and walkways to the accessible parts of the building. <p>Car Parking</p> <ul style="list-style-type: none"> » Car parking shall comply with AS 2890.1. » Disabled car spaces: minimum width of 3.8m, and should be signposted and painted blue, with a stencilled disabled symbol on the surface. » Disable parking rate: 3 spaces for every 100 car spaces provided, with minimum 1 disabled car space for car parks containing more than 12 car spaces. » Disable parking spaces to be as close as possible to entrances and exits, with access ramps provided. <p>Community Safety</p> <ul style="list-style-type: none"> » Configurations of buildings to allow natural surveillance onto public place. » Entrances designed to be open and visible from the street or public place. <p>Lighting</p> <ul style="list-style-type: none"> » Minimum 75 lux at a 1m height level. » Fluorescent lighting strips preferred, located approximately 2-2.5 metres above ground level located on building walls. <p>Pedestrian linkages</p> <ul style="list-style-type: none"> » Above ground pedestrian linkages between buildings or between buildings and car parks need to use attractive design, be well illuminated. 	<p>Complies – 8 disabled spaces are provided for the 275 car space facility. Parking spaces for people with a disability are indicated on architectural plans with bollards between spaces for extra clearance to 3.8m.</p> <p>Complies – refer to Section 5.2.3 of this SEE.</p> <p>Will comply – it is proposed that lighting design at detailed (CC) stage ensure good lighting across all areas of the site in accordance with advice from a CPTED Lighting specialist.</p> <p>Not applicable – However, a linkage between the adjacent existing and proposed car parks is proposed.</p>

Section	Control and performance criteria	Assessment
	<p>» Vibrant advertising on these structures is encouraged.</p> <p>Reflectivity</p> <p>» Low reflectivity glass encouraged.</p> <p>» Materials having a reflectivity index in excess of 20% will not be permitted.</p> <p>Shop Front Security Shutters</p> <p>» Preference is given to security grills to be installed behind shopfront. Shopfront security shutters are not favoured.</p> <p>» If used, shutters must be of an open type to allow pedestrians to view products in window displays.</p> <p>Solar Access</p> <p>» Solar access diagrams required for all developments where the site area exceeds 500sqm gross area or development exceeds two storeys in height.</p> <p>» Council will not approve structures that have a major shadowing effect on Freedom Plaza, Cook Square; the John Street pedestrian area, parks or other public pedestrian spaces and plazas within the Cabramatta Town Centre.</p> <p>Street Numbers</p> <p>» All buildings are required to have a street numbers that are easy to read by pedestrians and motorists.</p> <p>Walls to Street Frontages</p> <p>» Extensive blank walls to street frontages are not permitted.</p> <p>» Street frontages should contain shop fronts and activity.</p>	<p>Will comply – Glazing details at the CC design stage shall ensure low reflectivity glass is used.</p> <p>Complies – Individual awnings are proposed around the northern, eastern and southern perimeter of the building (above shopfront windows and /or doors) to protect pedestrians from the elements and to enhance the aesthetic appearance of the building. These awnings will be able to lowered so to enable a dual function as a security shutter at times when retail premises are closed. However, on the outside of the shopfront, these shutters provide an innovative and aesthetically advanced alternative to traditional roller shutters.</p> <p>Not applicable – The proposal provides two levels above ground however Level two is an open/rooftop level and therefore the building appears as a two story development. The proposal will have no impact on Freedom Plaza.</p> <p>Will comply – All shopfronts will be identified with a shop number.</p> <p>Complies – the proposal provided active uses (retail) along 3 of its 4 facades, which incorporate glazing and opportunities to extend active uses outdoors (e.g. outdoor dining). Excessive blank walls visible to the public realm are not proposed.</p>
General issues –	The total number of existing car parking spaces must not be	Will comply – The installation of temporary car spaces during the construction period will be

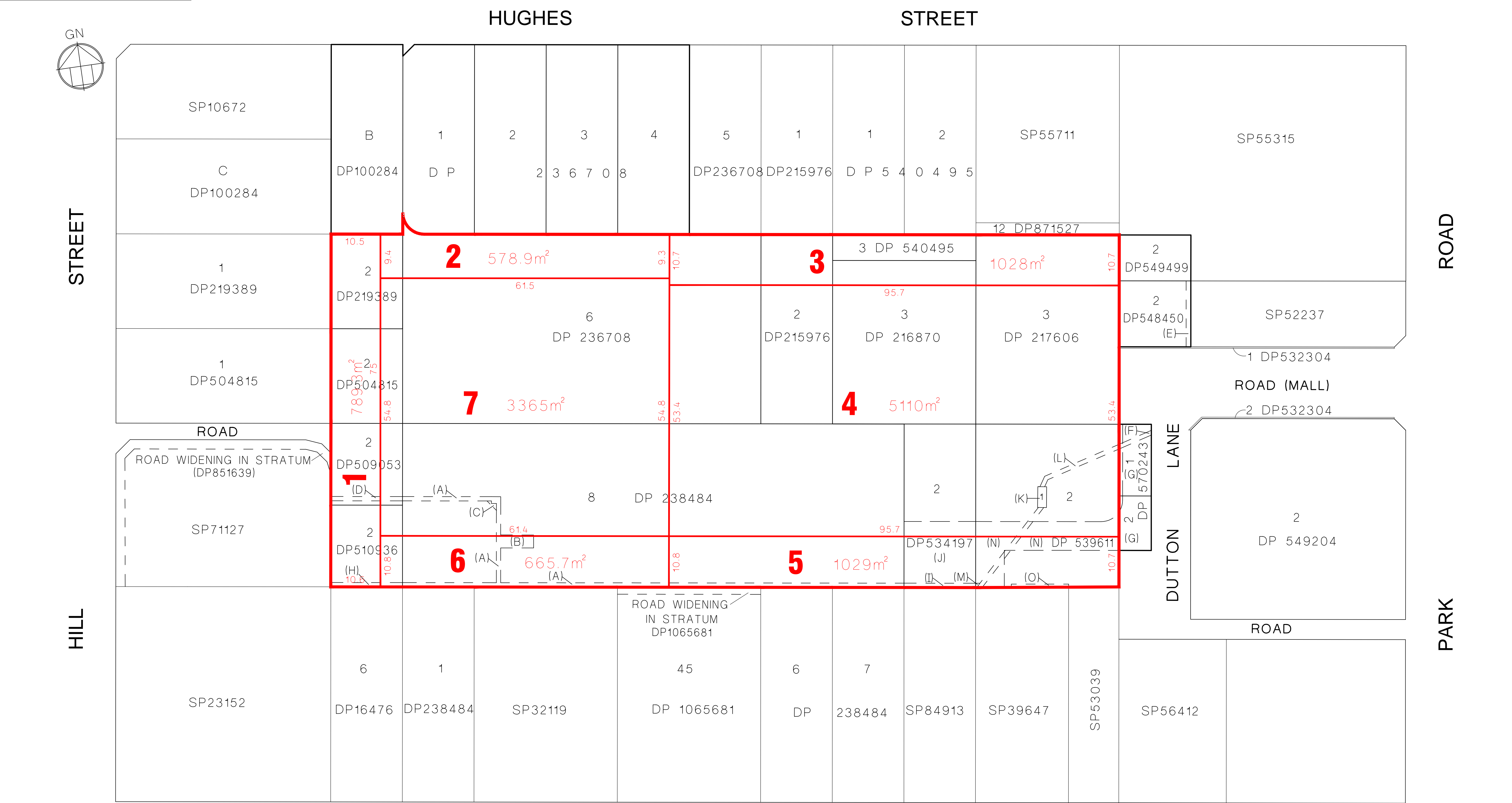
Section	Control and performance criteria	Assessment
Car parking	<p>reduced.</p> <p>For development proposed on one or more of the existing public car parks:</p> <ul style="list-style-type: none"> » Existing spaces must be relocated elsewhere on the same site, or » In a conveniently located position within the Town Centre, before the removal of any existing spaces. » Any existing public car parking spaces, if relocated, shall be re-established on the same side of the Town Centre as the existing location. » Replacement car parking for car spaces not reinstated on-site must be provided before the existing car parking facility is removed. <p>Where an existing car park, whether publicly or privately owned, is redeveloped and the car spaces are <u>not reinstated</u> on site, no Section 94 contribution discount may be applied to those car spaces not replaced on site.</p>	<p>addressed in a Construction Management Plan. All existing spaces will be reinstated once construction is completed and an Occupation Certificate is issued.</p>
General issues – Landscape principles	<ul style="list-style-type: none"> » Depending upon the scale of a particular development, consideration should be given to the following: <ul style="list-style-type: none"> > using deciduous trees to provide summer shade and winter sun and to soften and beautify any proposed development > the use of external planter boxes and potted plants where sufficient landscaping is not possible to enhance the design and soften the impact and appearance of buildings. > peripheral and central landscaping and the use of architectural treatments to reduce the visual impact of car parking areas. 	<p>Complies – the proposal makes provision for the introduction of soft landscaping (plantings) in the Dutton Lane precinct. This is considered a significant improvement given the current nature of the site, which is void of any landscaping or public domain features. The public open space will feature planter boxes able to accommodate plantings.</p> <p>Details of plants to be used in the public domain area shall be provided with the CC drawings.</p> <p>In line with CPTED principles, where plantings are proposed, the plant selections should ensure careful consideration of the size of plants (when mature) to ensure plants will not become places for attackers to hide.</p>

Section	Control and performance criteria	Assessment
	<ul style="list-style-type: none"> » Greater use of landscaping is encouraged to: beautify the town centre; reduce the visual impact of large buildings, and; provide a more amenable work and shopping environment. » Landscaping should correspond with the surrounding natural and built environment, and improve the aesthetics and appearance of the Cabramatta Town Centre. 	
General issues – Hours of operation	<p>The Cabramatta Town Centre operates as 7 day a week shopping and tourist centre.</p> <p>Uses will normally be permitted by Council to operate from 8am to 10pm 7 days a week.</p> <p>Trading outside these hours e.g.: night markets, retail activity, restaurants and clubs will be permitted by Council provided such extended hours would not adversely affect the amenity of any residential areas in close proximity.</p> <p>Council will require a submission of an acoustic report with a development application for any development proposal, which will operate outside the hours of 8am to 10pm and which may adversely impact on neighbouring properties.</p>	<p>Complies – The proposed development is unlikely to incorporate any uses that will be operating outside of the hours of 8am to 10pm, 7 days a week. Use/fit out of tenancies will be subject to future applications.</p>
General issues – Waste management	<p>It is the responsibility of the owner and/or tenant to ensure the regular collection and disposal of waste. Separate areas or rooms of adequate size must be provided for the storage of waste material (in individual bins or communal containers) and screened from public view.</p> <p>Access arrangements for service vehicles must be shown on applications.</p> <p>Recycling of waste material is encouraged.</p>	<p>Complies – The Architectural Plans illustrate a garbage room that is located on the ground floor and separated from other areas. The room's dimensions are 6.5m x 12.9m.</p>
Commercial outdoor	Controls for commercial outdoor eating/socialising on the	<p>Noted – Consideration needs to be given to the type of tenants that will be occupying particular</p>

Section	Control and performance criteria	Assessment
eating and socialising	<p>footpath/public spaces:</p> <ul style="list-style-type: none"> » Outdoor eating areas are permitted on non-porous pavement surfaces than can be kept clean. Outdoor eating areas are not permitted on asphalt/pavement areas, as they are difficult to keep clean. » The boundary of the footpath area must be marked by the use of landscape planter boxes, flowerpots, decorative low-scale fencing, or chain and post fencing. Clear unobstructed access of 3.0m must be provided. Where circumstances warrant a reduction to 2.5m may be supported. » Other operational controls relating to seating and furniture, umbrella's, advertising, waste bins, music, etc 	<p>shops and whether there will be a requirement for outdoor dining. The presence of outdoor dining may determine the type of external ground material used around the outside of the development. Outdoor dining space is provided at the eastern side of the building under awning treatment.</p> <p>Use/fit out of tenancies will be subject to future applications.</p>

B Architectural Plans

C Plan of Proposed Subdivision



SCHEDULE OF AFFECTING EASMENTS					
NOTATION	AFFECTING EASEMENT & CREATING DEALING/PLAN	BURDENED LOT	NOTATION	AFFECTING EASEMENT & CREATING DEALING/PLAN	BURDENED LOT
(A)	EASEMENT FOR ELECTRICITY PURPOSES (P64258)	8/238484	(I)	EASEMENT FOR UNDERGROUND MAINS (P584548)	2/534197
(B)	EASEMENT FOR ELECTRICITY PURPOSES (Y208087)	8/238484	(J)	RIGHT OF WAY (P477997)	2/534197
(C)	EASEMENT FOR UNDERGROUND MAINS (Y208087)	8/238484	(K)	EASEMENT FOR ELECTRICITY PURPOSES (3475361)	1/539611
(D)	EASEMENT FOR UNDERGROUND MAINS (P64259)	2/509053	(L)	EASEMENT FOR UNDERGROUND MAINS (M27667)	2/539611
(E)	EASEMENT FOR OVERHANGING EAVE & AWNING (SP52237)	2/548450	(M)	EASEMENT FOR UNDERGROUND MAINS (P64259)	2/539611
(F)	EASEMENT FOR UNDERGROUND MAINS (M28960)	1/570243	(N)	RIGHT OF WAY (P532693)	2/539611
(G)	RIGHT OF WAY (P532693)	1 & 2/570243	(O)	EASEMENT FOR OVERHANGING EAVE DP645138)	2/539611
(H)	EASEMENT FOR UNDERGROUND MAINS (P64259)	2/510936			

DRAFT

FOR DISCUSSION PURPOSES

PLAN OF PROPOSED SUBDIVISION

SUBDIVISION OF LOT 2 DP 219389, LOT 2 DP 504815, LOT 2 DP 509053, LOT 2 DP 510936, LOT 6 DP 236708, LOT 8 DP 238484, LOT 2 DP 215976, LOT 3 DP 540495, LOT 3 DP 216870, LOT 2 DP 534197, LOT 3 DP 217606, AND LOT 2 DP 539611
DUTTON LANE, CABRAMATTA

Scale : 1:400 @ A1 Date : 07/08/2013 Plan No. : 23123-SUB 1 Issue : A

Client : FAIRFIELD CITY COUNCIL

D Traffic Impact Assessment

E Economic Impact Assessment

F Architectural Statement

Architects Statement

Dutton Lane Car Park Redevelopment

AA.MIX.1301

Date : 16.09.2013

Design Approach

Located on Dutton Lane in Cabramatta Town Centre, the proposed redevelopment is designed to provide an excellent amenity for the community as well as a pleasant and positive contribution to the site and surrounding environment.

The development consists of retail use at ground level, some commercial space at the first level and a public car park on the first and second level.

Through focus on design excellence, the proposal would elevate Dutton Lane whilst also giving the Cabramatta Town Centre a landmark building.

Design response

The proposal allows for a dialogue between the proposed built form and its context. This provides the development with a concept plan that creates visual connectivity that would reinforce the experience and amenity of the precinct.

Viewed from across the site the development would demonstrate legibility with regards to scale and context whilst when moving through the site the homogeneity of both colour and materiality provides visual continuity and cohesion to the development.

The proposal seeks to reinforce and integrate connectivity across the site to adjoining and existing open spaces and arcades. By creating malls that respond to the existing movement patterns, and with ground floor retail enjoying a dual frontage to both street and mall, built form transparency and activation is expressed.

Linkages to Freedom Plaza through the location of mall and public open space, adds to the legibility of both movement and pedestrian access.

The design proposal incorporates a public open space that provides opportunities for passive recreation, integration with ground floor shops, and connection to Freedom Plaza

Built Form

This Proposal provides a unique opportunity to create an architectural language evolved from respect for the site's built context. Our response has been to develop a modern and contemporary language that is contextual and identifiable.

Materiality

The colours and materials have been carefully selected to complement both the architectural expression as well as its surroundings. The colours chosen are shades of natural palettes, that being an earthy red and grey, as well as the use of natural off form concrete, which provide a soft yet bold identity to the building, allowing it to act as a landmark within its context.

The overall concept provides a contemporary and strong architectural expression, which reflects and sits well within its context and surrounds. The development also fits well to the proposed scale and desired character of the precinct.

Elevation Treatment

The composition of the proposed facade is primarily made up of Aluminium panels and off form concrete walls. The Aluminium panels are perforated with a regulated pattern to create a visual interest across the facade. The materials have been selected for their robustness and durability which are desirable characteristic for a public building of this nature.

The proposed colours to be used in the facade are in tones of earthy Red to provide a softer and natural nuance to the carpark, whilst the concrete will remain in its natural colour with a honed finish.

Facade Design Detail

The facade system contains two key elements - off form concrete walls and perforated aluminium panelling fixed to steel framing. This system has been designed so that most prefabrication are done off-site to reduce on-site welding and fabrication which would improve construction efficiency significantly.

The façade has also been designed to comply with the specific requirements of the BCA of providing fall protection as well as eliminating any foot-hold so that it can not be climbed.

The aluminium panel have patterned perforation to provide right amount of balance of screening to the car park as well as visual permeability to provide security, almost acting like a veil to the building. The perforated panels are deconstructed around the commercial space to enhance visual interest as well as allowing stronger transparency and visual connection from the commercial tenancy. Entrances to the retail on ground level are signified by expressing the screen further in a way such that the screen becomes deeper in profile. This design of panels folding at entrances also provides opportunity for additional lighting to penetrate through from the car park.

Lighting

Energy efficient fittings are proposed throughout the car park and used in conjunction with photocell and time switches to reduce energy consumption.

The external roof level will be fitted with unobtrusive lighting to prevent light spilling onto the adjacent properties. All lightings are to meet Australian Standard for Car parks.

Stair wells

The proposed development provides four fire-isolated stairways as per BCA requirements. These stairwells form a break in the building's façade and contribute to the visual interest of the building. The stairs are open to the street providing visibility for natural surveillance and assists as a crime prevention strategy.

Conclusion

The design proposal evolved out of a process, which has reference to various relationships between the site and its context. The process reviewed the site in a holistic and inclusive manner, and has resulted in the creation of a unique and contemporary architectural expression that is contextual and identifiable in the existing context and surrounds. The development also enhances the quality of the environment and surveillance as well as provides excellent amenity to the community.

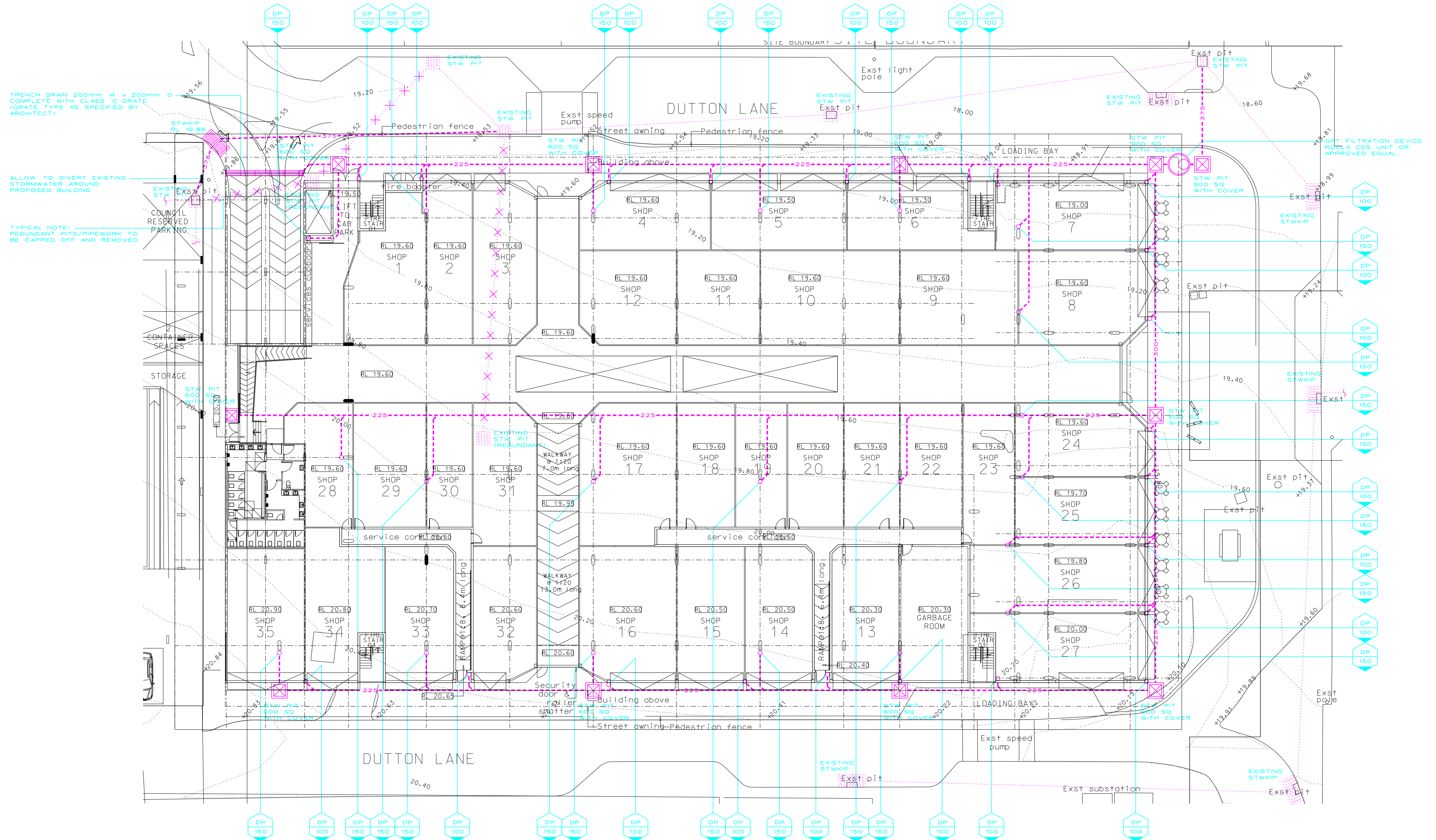
G Stormwater Drainage Plan

STORMWATER DA
DRAWING ONLY

- NOT FOR CONSTRUCTION
- FINAL LOCATION OF ALL DOWNPIPES, PITS, RAINWATER OUTLETS
AND SUBSOIL PIPES TO BE CONFIRMED DURING CONSTRUCTION
CERTIFICATE STAGE OF THE PROPOSED DEVELOPMENT

NOTES

FOR GENERAL NOTES AND LEGEND REFER
TO COVER SHEET



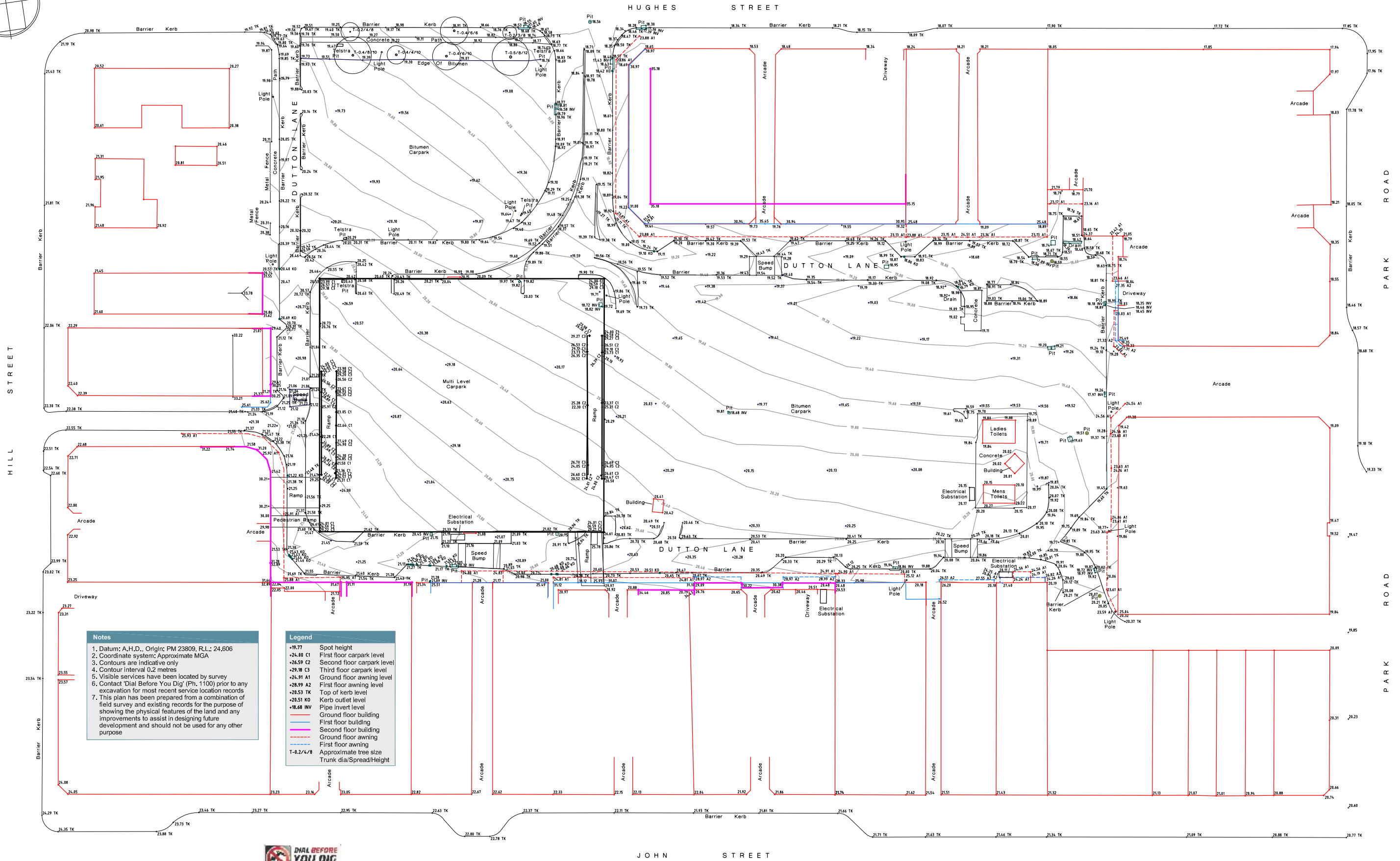
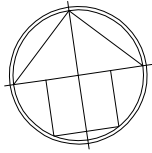
		LIST OF REFERENCE DRAWINGS USED AS BACKGROUND		DRAWING REVISION		CLIENT FAIRFIELD CITY COUNCIL		ARCHITECT ANTONIADES ARCHITECTS		PROJECT DUTTON LANE CARPARK REDEVELOPMENT CABRAMATTA		DRAWING TITLE GROUND FLOOR STORMWATER DRAINAGE	
		DISCIPLINE	COMPANY	DRAWING No	A							SCALE 1:200 at A1	
		ARCH	ANTONIADES	DA 1.01	K							JOB No 12/216	
		SURVEY	?	23123.DETAIL	?							DISCIPLINE HYD	
		STRUCT										DRAWING No H-DA-01	
		ELEC										REVISION B	
		FIRE											
B ISSUED FOR DA		24.07.13	MECH										
A ISSUED FOR DA		19.07.13	ELEC										
REV DESCRIPTION		DATE											

0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

STW DA DRAWING

H BCA Assessment

I Survey Plan



J Advertising for Community Information & Feedback Session

Tái xây dựng đường Dutton

BUỔI THÔNG TIN CỘNG ĐỒNG

THỨ NĂM 05/09/2013

5.30pm - 8.30pm

Cabravale Leisure Centre
Broomfield St, Cabramatta

Hội đồng thành phố Fairfield đang tổ chức một buổi thông tin cộng đồng để cập nhật với cộng đồng về việc tái xây dựng của đường Dutton, Cabramatta

- Trình bày các bản thiết kế và hình mẫu của dự án được đề xuất
- Gặp gỡ các đại diện của nhóm dự án để thảo luận về dự án
- Cung cấp thông tin phản hồi cho nhóm dự án trước khi nộp đơn xin xây dựng



Buổi thông tin này là một chương trình bổ sung được cung cấp bởi Hội đồng nhằm thông báo một cách tốt hơn đến cộng đồng về việc tái xây dựng này.

Một buổi triển lãm công cộng chính thức cũng sẽ được tổ chức như là một phần của tiến trình xây dựng sau khi đơn xin xây dựng đứng chính thức nộp lên.

Để biết thêm thông tin, xin vui lòng liên lạc Hao Dang
tại số 9725 0222

Dutton Lane 重建工程計劃

社區諮詢會議2013年9月5日

週四5.30PM—8.30PM

Cabravale Leisure Centre, Broomfield St, Cabramatta

菲費市政府將舉辦一次社區諮詢會議向社區公佈有關建設Dutton Lane, Cabramatta的最新消息。

*參觀設計圖則和一座模擬工程模型

*與工程項目組代表會面並討論有關建造計劃

*在工程項目遞交申請前向工程項目組反饋意見。

13061210LT 130828ZL Zin

諮詢會議是市政府為社區公眾能更進一步瞭解建設工程。

作為建設程序的一部份並且在項目申請確立後，建設項目公眾展覽會也將照常開放。

有關詳情查詢，請電Hao Dang 02 9725 0222

COMMUNITY INFORMATION SESSION



Redevelopment of Dutton Lane Car Park

Thursday, 5 September 2013

5.30pm - 8.30pm

Cabravale Leisure Centre, Broomfield St, Cabramatta

Fairfield City Council is holding a Community Information Session to update the community about the proposed development of Dutton Lane Car Park Cabramatta.

- View design plans and a model of the proposed scheme
- Meet representatives of the Project Team to discuss the proposed development
- Provide feedback to the Project Team prior to lodging a development application

The Information Session is an additional step provided by Council to better inform the community of the development.

A formal public exhibition process will still be undertaken as part of the development process after the application has been formally lodged.

We look forward to meeting you at the Information Session.

For more information,
contact Mr Hao Dang on 9725 0222

News

Online Content

Champion

Exclusive stories @ our website



Thursday: An extra \$400,000 of federal funding will help give the South West Sydney Legal Centre a fair go.



Friday: Community radio station 2GLF 89.3 FM will open their new office on Saturday after a successful 30 years of broadcasting.



Monday: Women living in Sydney's south-west will earn far less than women in Sydney's wealthier suburbs under Opposition Leader Tony Abbott's paid parental leave scheme.

fairfieldchampion.com.au



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LIKE us on Facebook to join in the chat with fellow residents and interact with *Champion* staff. Each day there'll be something new to get you through the working day.

This week ...



Tuesday

Caption This! - This is where you get to play editor and add a caption to our pic of the week.

Wednesday

Your favourite 5 - Your chance to share with fellow readers your "favourites" across a range of topics. This week, your favourite five Smurfs.

Friday

Word change game - A bit of fun where you take one word from a song title and replace it with another. This week we replace the word "love" with "loathe".

enZOS
CUCINA

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KEMPS CREEK



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- Communion
- Confirmations
- Engagements
- 21st
- Catering
- Outside Functions

Enzo Marturano

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Shops 9 & 10 Chipping Norton Market Plaza,
Crn Barry Road & Ernest Avenue Chipping Norton

Bookings 9726 8142



David Risteski & Joey Pavlovich

7 Nights • Lunch Sunday

2/795 Fifteenth Avenue Kemps Creek

Bookings 9606 9900

www.enzoscucina.com.au



Have your say at:
<http://haveyoursayfairfieldcity.com.au/>

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 Celebrating diversity



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NEWS IN BRIEF



Mayor Frank Carbone says 'Yes' for Fairfield.

LOCAL GOVERNMENT REFERENDUM OFF

The referendum to financially recognise Local Government in the Australian Constitution will no longer proceed alongside the upcoming Federal Election. Prime Minister Kevin Rudd announced the Federal Election will take place on 7 September, so plans to hold the referendum have been cancelled. The need for Local Government to be recognised in the Australian Constitution remains a priority.



Miguel Maestre will cook four of his signature dishes.

MIGUEL COMING FOR CULINARY CARNIVALE

Celebrity chef Miguel Maestre will be the main attraction at this year's Culinary Carnivale in Fairfield City Centre on 19 October. Miguel will be cooking four of his famous signature dishes, including paella and churros. Culinary Carnivale will feature the delightful tastes of Spanish and Latin American dishes as well as traditional dance and music. It is part of this year's Good Food Month. For more information visit www.fairfieldcity.nsw.gov.au.

PUBLIC EXHIBITIONS

Notice of Development Applications Received

Proposal: Use of Ground Floor as a Community Centre with a Place of Worship

Premises: 3 Howat Street, 5 Howat Street & 890 Woodville Road, Villawood

Lot Description: Lot 1 DP: 106243, Lot 31 & 32, DP 1073955

Applicant: Mr S I Hussain

Application Number: 501.1 / 2013

Consent Authority: Fairfield City Council

Development application will be on exhibition from 29 August 2013 to 18 September 2013 at Council's Administration Centre, 86 Avoca Road, Wakeley between 8.30am to 4.30pm (Monday to Friday). Submissions may be made to the City Manager, in writing until 4.30pm on the final day of the exhibition period.

You can also comment online via Council's website by clicking the green 'Have your Say' button and go to 'Development Applications on Exhibition'. If you wish to make a submission, Council's file number should be quoted. All submissions made



The noodle eating contest is a popular Moon Festival event.

MOON FESTIVAL TWO WEEKS AWAY

Cabramatta's 15th annual Moon Festival is a little over two weeks away and boasts a stellar line-up of entertainment. This year's headline acts will feature popular children's entertainer Peppa Pig, Alvin Quah (2010 Channel 10 MasterChef contestant) and Butterfly Skye – an entomologist who grows bugs such as scorpions for food! The fun starts at 11am on 15 September and ends at 8pm with the Bupa Fireworks.



Dutton Lane Car Park in Cabramatta.

DUTTON LANE INFORMATION SESSION

Council will hold a community information session about the proposed development of Dutton Lane Car Park in Cabramatta at Cabramatta Leisure Centre on 5 September between 5.30pm and 8.30pm. Residents will have an opportunity to view design plans, meet members of the project team and provide feedback before the lodgement of a development application. Contact Hao Dang on 9725 0222 for more information.

will be considered prior to a decision being made.

For any further enquiries in relation to any of the applications, please call 9725 0222.

In accordance with the provisions of the Government Information (Public Access) Act 2009 any comments and / or objections received will not be kept confidential and become open access information.

Public Exhibition of Fairfield City Council

Draft Policy 3-104

Councillor Expenses and Facilities For 2013/14

Reference No. 11/03271

Council at its meeting on 27 August 2013 endorsed for public exhibition, a draft policy for Councillor Expenses and Facilities in accordance with the requirements of the Local Government Act 1993 and guidelines issued by the Division of Local Government. The draft policy will be on public exhibition at Council's Administration Centre, Wakeley between the hours of 8.30am

WHAT'S ON

THURSDAY BIKE RIDES

Thursdays,
 9.30am-12pm
 WSCN Bike Shed,
 Fairfield Showground,
 Prairiewood
 Cost: Free
 Phone: 9724 1520

COMPUTER CLASSES

Mondays @ Bonnyrigg
 Library
 Tuesdays @ Whitlam
 Library, Cabramatta
 Wednesdays @ Fairfield
 Library
 Thursdays @ Wetherill
 Park Library
 10am-12.30pm
 Cost: \$5
 Phone: 9725 0344

EXHIBITION - WARNING

(MAY CONTAIN NUTS!)
 Saturday, 24 August -
 Sunday, 12 October
 Fairfield City Museum
 and Gallery
 Cost: Free
 Phone: 9609 3993

CABRAMATTA MOON FESTIVAL

Sunday, 15 September,
 11am-8pm
 Cabramatta Town Centre,
 John Street, Cabramatta
 Cost: Free
 Phone: 9725 0222

COUNCIL MEETINGS

10 SEP Outcomes Committee
 Staff Luncheon
 Services Committee
 Committee Rooms

12 SEP Independent
 Development
 Assessment
 Committee
 Staff Luncheon

24 SEP Council Meeting
 Council Chambers

Please contact Council on 9725 0222 on the day of the meeting to confirm the starting time.

Continued on page 12

to 4.30pm Monday to Friday, from Wednesday, 28 August until Wednesday, 25 September 2013. Submissions must be received by 4.30pm on Wednesday, 25 September 2013.

Public Exhibition of Planning Proposal

Draft Amendment to Fairfield Local Environmental Plan 2013 to Permit Secondary Dwellings (Granny Flats) in Ru2 - Rural Landscape and Ru4 - Primary Production Small Lots (Horsley Park & Cecil Park)

Council at its meeting held on 28 May 2013 resolved to publicly exhibit a draft Planning Proposal to permit secondary dwellings with consent in the rural area of the City (draft Amendment No. PP_2013_FAIRF_002_00) in accordance with the Department of Planning and Infrastructure's Gateway Determination dated 1 July 2013.

As part of the Gateway Determination, the Minister has delegated his plan making powers to Fairfield Council to exercise delegation in the making of this Planning Proposal.

Wakeley Administration Centre 86 Avoca Road, Wakeley **Customer Service** 9725 0222 Monday to Friday, 8.30am - 4.30pm
Mailing Address PO Box 21, Fairfield NSW 1860 mail@fairfieldcity.nsw.gov.au

FAIRFIELD

Film to feature journeys

TRAVEL TALES WILL INSPIRE

Kimberley Caines

LISTEN directly to the voices and see through the eyes of students from all corners of the world in a short film about journeys titled *Travel Songs of Sea and Land*.

More than 35 students from Fairfield High School and Fairfield Intensive English Centre collaborated with Shopfront artists and three other schools during the past five months to create music, dance, animation and visuals for the 30-minute film.

"The short film is about journeys, whether it is imaginary or every day journeys," Shopfront outreach director Hannah Grant said.

A diverse mix of students including refugees and new migrants, young people with disabilities, and those who have grown up in suburban Sydney shared their personal, physical and emotional journeys.

"Some of the students from Fairfield are newly arrived to Australia and had



Some of the Fairfield High students featured in the short film *Travel Songs of Sea and Land*.

Picture: TIM CLAPIN

interesting stories to tell," Ms Grant said.

"They made a collage of their faces which showed different pieces of who they are."

The film is about various travel stories such as a young man who is running late for his train; two friends

who say a final goodbye at an airport; and a woman who wades out to sea while an Iraqi love song calls out.

Co-artistic director Caitlin Newton-Broad said the film would help viewers walk in the student's shoes.

"Young people care about home, belonging and protec-

tion and they use their optimism and resilience to face big change and some people have had powerful experiences which they shared," she said.

The film will be shown at various western Sydney venues from tomorrow until Sunday.

NOW SHOWING

What: 100 students shared their journeys in a short film
Where: Hazelhurst Gallery, Hurstville Library Foyer, Bankstown Arts Centre
When: September 5-8
Details: 9588 3948, projects.shopfront.org.au

FAIRFIELD AND SUBURBS

Get involved in Balloon Day

FAIRFIELD residents are preparing to play their part in raising awareness and funds for children in need for Bravehearts annual White Balloon Day on Friday, September 6.

Research shows that one in five Australian children are sexually assaulted before their 18th birthday. To make Australia a safer place, get involved and register as a volunteer at whiteballoonday.com.au.

CANLEY HEIGHTS

Community crafts in town

CANLEY Heights Town Centre will come alive with colour and variety at the community craft market this Saturday from 10-2pm.

The market will feature a variety of unique handmade items such as jewellery, ceramics, knitwear and art.

The aim of the market is to provide opportunities for local community groups with artistic and creative skills to showcase and sell their products.

For more information, contact 9823 7542.

Redevelopment of Dutton Lane Car Park COMMUNITY INFORMATION SESSION

THURSDAY, 5 SEPTEMBER 2013
5.30PM-8.30PM

Cabravale Leisure Centre • Broomfield St, Cabramatta

Fairfield City Council will hold a Community Information Session to update the community about the proposed development of Dutton Lane Car Park Cabramatta.

- View design plans and a model of the proposed scheme.
- Meet representatives of the Project Team to discuss the proposed development.
- Provide feedback to the Project Team before lodgement of a development application.

The Information Session is an additional step provided by Council to better inform the community of the development.

A formal public exhibition process will still be undertaken as part of the development process after the application has been formally lodged.

We look forward to meeting you at the Information Session.

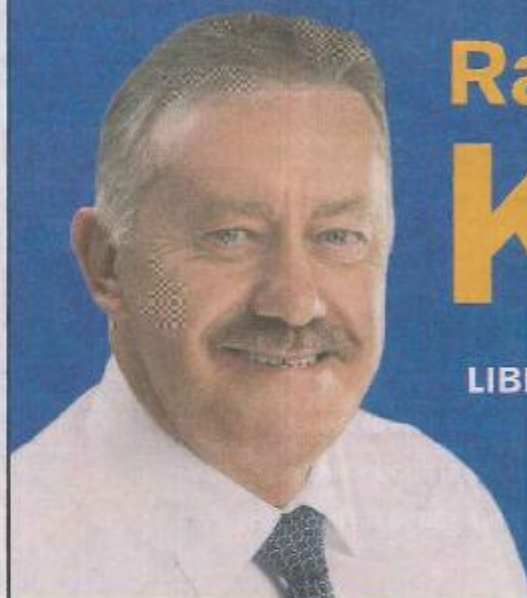
For more information, contact Mr Hao Dang on 9725 0222.



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ADVERTISEMENT

More jobs Stronger borders



Ray
King

LIBERAL FOR McMAHON

Read Our Plan at:
realsolutions.org.au

Authorised by Mark Newton, 103 William Street, East Sydney NSW 2011

K Display Boards at Community Information & Feedback Session

01

INTRODUCTION

Welcome to Fairfield City Council's Community Information Session for its Dutton Lane at-ground car park redevelopment project.



The Community Information Session has been voluntarily organised by Council to inform you about its redevelopment proposal. There will also be a formal public exhibition period as part of the development assessment process should a development application for the proposal be lodged.

The proposal is an important part of Council's vision to renew Cabramatta town centre, better utilise its asset and continue to improve its public amenities.

The proposal will form the basis of a development application which will be assessed and determined by the Joint Regional Planning Panel later this year.

Council representatives are on hand to talk with you and answer questions you may have about the information on display.

We thank you for your interest in the project.

DUTTON LANE
AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

02

PROJECT OVERVIEW



The development proposes the construction of a three storey building with retail uses at ground level, some commercial space at the first level and a public car park on the first and second level.

The redevelopment plan is expected to:

- > Generate an estimated \$16Million for the local economy during construction
- > Support an estimated 127 full time and part time jobs in retail and commercial services
- > Create 64 jobs directly and 199 indirectly, for a total of 263 jobs during the construction phase
- > Increase shopper convenience by providing a new car park with 74 additional on-site parking spaces than currently available
- > Improve pedestrian and vehicular interface and safety at Dutton Lane
- > Improve ingress and egress from the car parks and queuing time.

The development proposes the construction of a 3 storey building with:

- > Retail uses at ground level
- > Commercial space at Level 1
- > Public Car Park on Level 1 and 2
- > Public outdoor space and seating.

\$46 Million

of total economic activity
will be generated by the
construction of the
proposed development.

DUTTON LANE

AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

03

ABOUT THE SITE

The site is located in the heart of the vibrant and busy Cabramatta Town Centre. Physical improvements to the centre should continue to support its vitality.

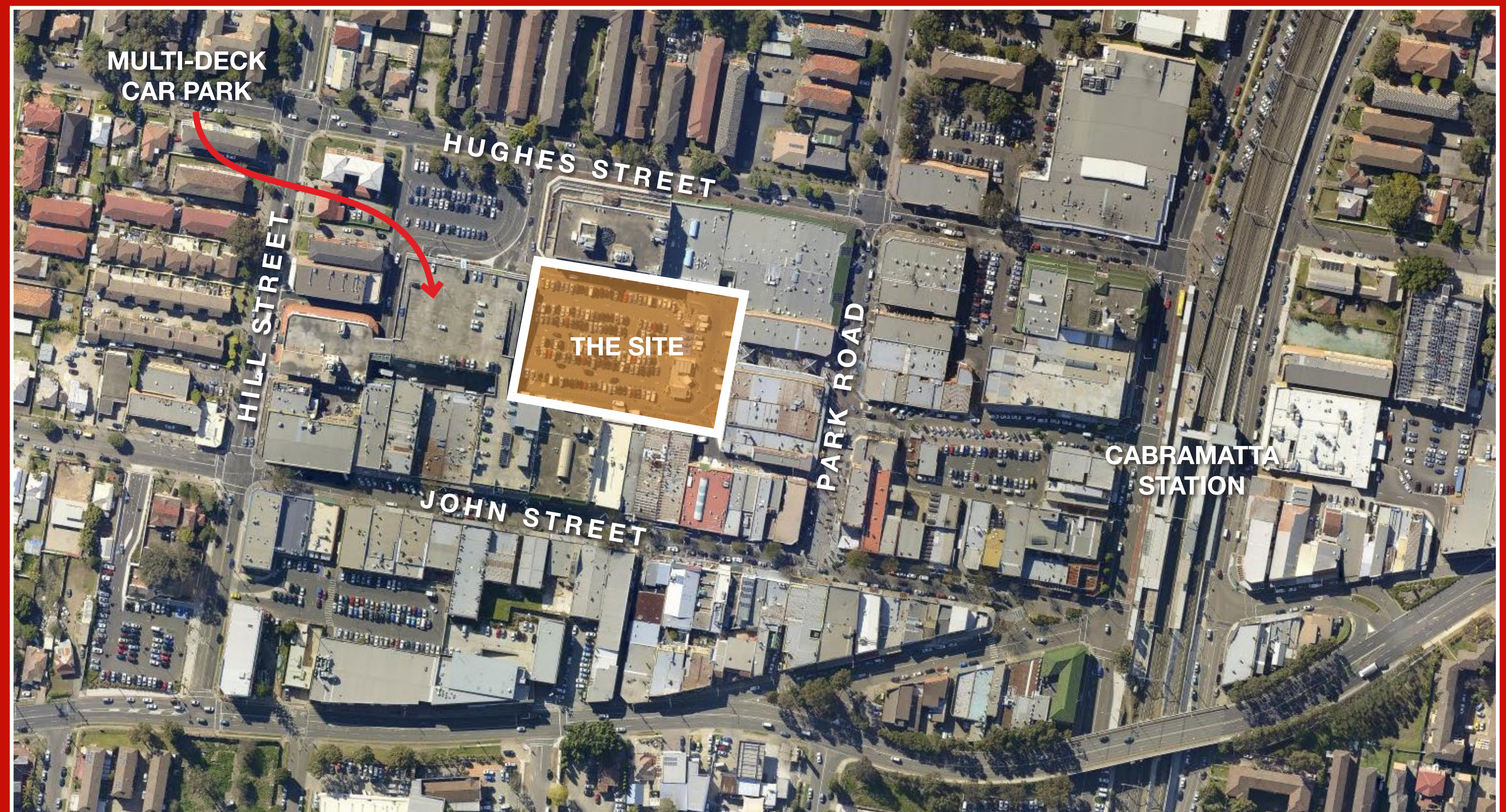
The site is Council owned and currently used as an open ground-level car park providing 157 spaces. There is an existing toilet facility located in the south east corner.

The site is surrounded by retail activities to the north, east and south. A 3 storey car park building is on the neighbouring site to the west.

Access to the site is via Dutton Lane from Hughes Street. There is opportunity to access Hill Street from Dutton Lane.

Pedestrians currently move across Dutton Lane and the car park in a dangerous, ad hoc manner, with only informal links to the surrounding shopping centres.

The current arrangement of loading bays in Dutton Lane creates an unsafe environment for people walking across the roadway and for cars to drive around.



DUTTON LANE

AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

04

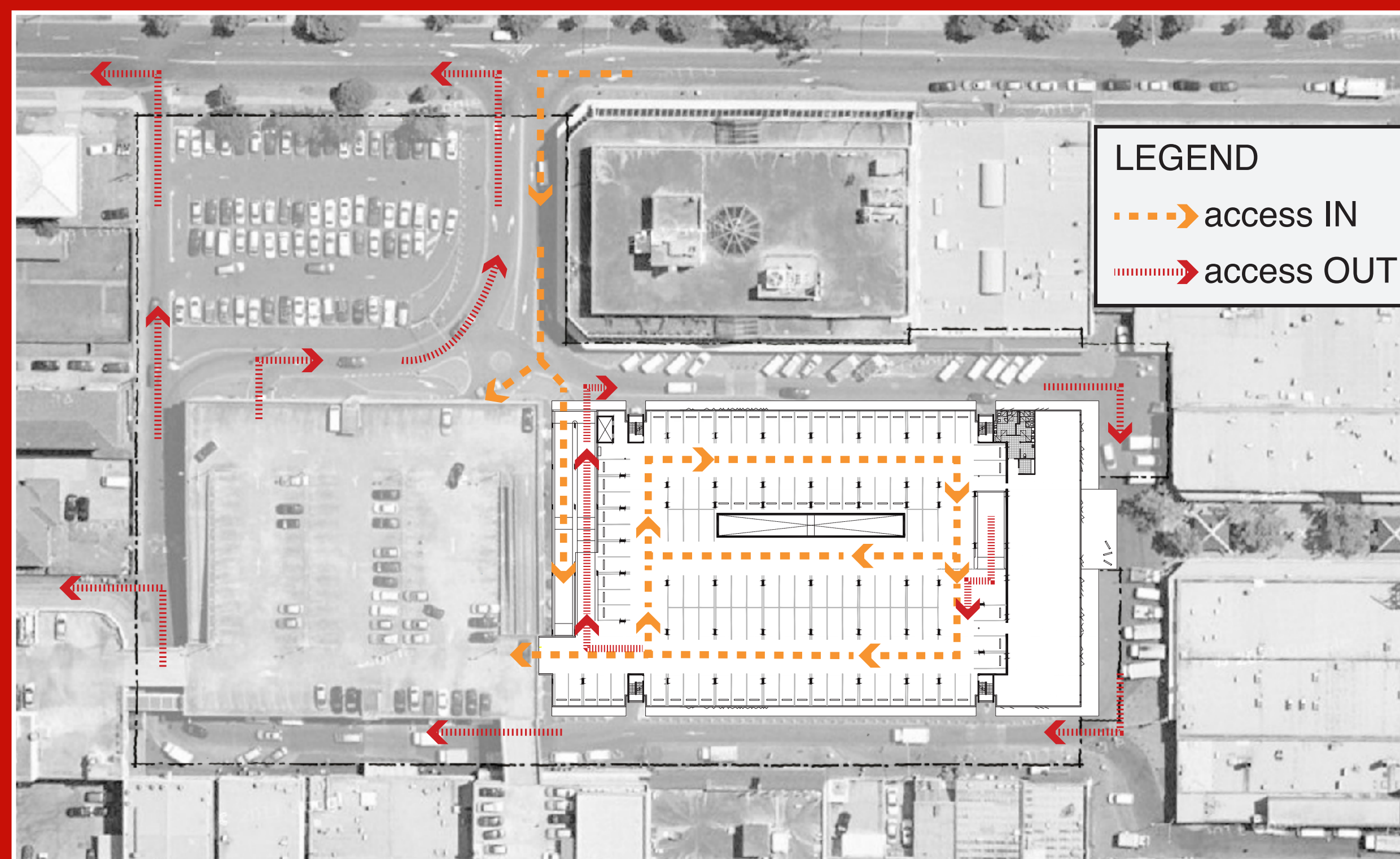
WHAT IS BEING PROPOSED

Redevelopment of the Dutton Lane at-ground car park into a multi-level car park will provide:

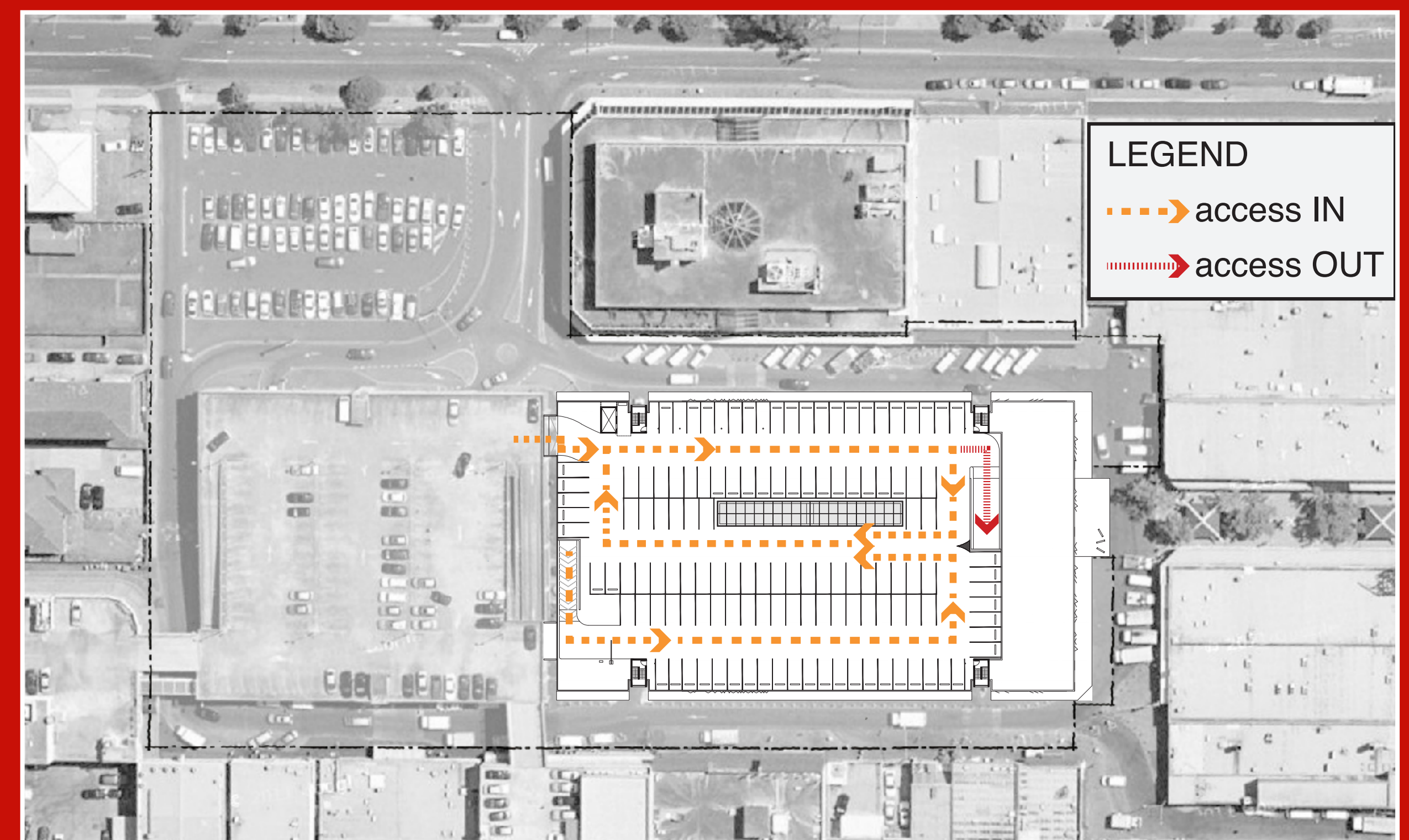
- 1** ▶ **240 car parking spaces** over 2 levels above retail spaces on the ground floor.
- 2** ▶ Access to the new car park via a **new express ramp** from Dutton Lane.
- 3** ▶ **A new lift to provide access to car park levels** above ground.
- 4** ▶ **A new public outdoor space** to the east of the new building providing public seating, outdoor activities and dining.
- 5** ▶ **A new public toilet facility** on the ground level of the new development to replace the existing toilet block.
- 6** ▶ **A connection to the adjoining Council multi-storey car park** allowing patrons to connect easily between both car parking stations.
- 7** ▶ **Improvements to the intersection of Hughes Street and Dutton Lane** to a formalised 'seagull' arrangement. No major civil work is required.
- 8** ▶ **2,995m² of retail space at ground level** fronting Dutton Lane and an internal arcade.
- 9** ▶ **505m² of commercial space** on the first floor to be retained by Council for Council offices or community uses.
- 10** ▶ **Parallel loading bays along Dutton Lane** and some bays located on the southern side of Hughes Street between Park Road and Dutton Lane East.
- 11** ▶ **Raised marked pedestrian crossings** and pedestrian fencing around the new building to make pedestrian movement safer.
- 12** ▶ **The project will be 100% developed,** funded and owned by Fairfield City Council.

05

TRAFFIC AND PARKING



Level 1



Level 2

The surrounding road network, including Park Road and Hughes Street, has the capacity to accommodate the traffic generated by the new development.

74 additional spaces will be created in the car park.

The number of existing loading bays – 37 in total – will remain and 1 new loading bay will be created. To improve safe movement in Dutton Lane some will be reconfigured to parallel parking and 6 parallel truck spaces will be relocated to Hughes Street. 9 car spaces on Hughes Street which will be removed to accommodate the loading bays will be reinstated in the new car park. This solution provides significant improvements to Dutton Lane and minimises the amount of heavy vehicles entering the Lane.

Access to the car park will be near the access lanes to the neighbouring multi level car park to allow for a safe traffic management. It is anticipated that a majority of vehicles exiting the car park will do so via the adjoining multi-deck car park which provides connection to Hughes Street via Dutton Lane East. As such, the traffic demand associated with the new car park is expected to have limited additional car movements on Dutton Lane.

The internal one way nature of Dutton Lane minimises the potential for car accidents or vehicle pedestrian accidents.

Traffic modelling and analysis has found that significant improvements to vehicular movements in and around Dutton Lane can be achieved if the above proposed arrangements were adopted.



DUTTON LANE

AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

74
additional
car spaces
will be created

06

PEDESTRIAN REALM

Design of the development creates a safe and pleasant environment for those working at, or visiting, the site.

The general standard of Dutton Lane for pedestrians will be significantly improved as the proposed development will:

- > Improve existing pedestrian connections by formalising pedestrian routes that link the development to existing shopping malls and arcades
- > Install decorative fencing along footpaths to direct people towards raised marked pedestrian crossings lane, thereby minimising pedestrian / vehicle conflict
- > Remove the existing non-compliant 90 degree angled loading bays around high pedestrian areas.

Arcades at ground level will provide good connections to existing arcades and malls. The creation of a public outdoor space to the east of the new building will provide a link to Freedom Plaza. This space will include public seating, landscaping and lighting.

The redevelopment has been designed to create a safe space by:

- > Creating attractive spaces across the site to entice people and ensure that areas remain well used
- > Maximising the opportunity for surveillance during day-to-day activities
- > Enhancing access control by providing lighting within the public domain to guide people to preferred safe routes
- > Providing a clear distinction between public and private space
- > Ensuring the area is well maintained, which sends a clear message that people care about and notice what happens in the area.



DUTTON LANE
AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

07

RETAIL AND COMMERCIAL SPACES

**127
permanent
jobs**

are estimated to
be created post
construction

The project will set a new benchmark for retail development in Cabramatta.

The redevelopment responds to growing demand for retail space in Cabramatta, and will attract capital investment to improve the viability of the Town Centre.

The development will result in positive community benefit in the Town Centre by replacing the at-grade car park with active vibrant retail space.

The retail spaces at ground level can be used for cultural, retail, business, tourist or entertainment activities. The future uses will complement existing businesses in the Town Centre.

This development will work with Council's Town Centre Improvement Program upgrades to enhance the bustling Cabramatta CBD and improve the aesthetics and enjoyment of Dutton Lane.

The commercial space on the first floor level is envisaged to be used by Council for its services and community uses.

The proposed development will be operated and fully owned by Council.

The proposal would generate an estimated 127 permanent jobs post-construction.



DUTTON LANE

AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

08

PLANNING PROCESS

History

Council resolves to start the redevelopment process by appointing Antoniades Architects to prepare designs

Early planning by project team working with Council to produce preliminary plans and traffic studies

Meetings with Council and Cabramatta Police to discuss key planning issues

Current

Community and stakeholder consultation

Redevelopment plans will be finalised by project team

Development Application will be lodged to Fairfield City Council

Development Plans will be placed on public exhibition for 28 days

Assessment of the proposal will be carried out by an independent external town planning consultant

The proposal will be considered and determined by the JRPP

WE ARE HERE

The Joint Regional Planning Panel (JRPP) will be the determining authority for the proposal, not Council, because the project has a Capital Investment Value of over \$5 Million. The elected Council will have no role in approving, authorising or endorsing the assessment report. Council ensures that the delivery of this proposal adheres to best practice principles, probity guidelines, transparency and accountability.

Construction is likely to start in late 2014 subject to approval

Future

DUTTON LANE

AT-GROUND CAR PARK
REDEVELOPMENT PROJECT

09

NEXT STEPS

Thank you for attending today's community information session.



Following consideration of comments received at this information session, Council will finalise the plans for the Dutton Lane Car Park redevelopment that will inform the Development Application for the proposed changes.

If you would like to provide Council with any comments regarding the Dutton Lane Car Park please fill in a feedback form and place it in the feedback box as you leave today.

You will also have an opportunity to comment on the proposed changes during the formal public exhibition of the Development Application later this year.

Thank you.

DUTTON LANE
AT-GROUND CAR PARK
REDEVELOPMENT PROJECT



www.elton.com.au